



NORTHWEST
◀ CROSSING ▶

PROTOTYPE HANDBOOK

WEST BEND PROPERTY COMPANY LLC

FEBRUARY 2015



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Project Background

West Bend Property Company LLC was formed in April 1999 to master plan a 486 acre undeveloped tract on the west side of Bend. NorthWest Crossing is within the Bend city limits and was zoned by the City of Bend for residential, commercial and industrial uses. West Bend Property Company LLC spent considerable time working with a team of planners, designers, engineers and economists to create the master plan for NorthWest Crossing. The team worked to ensure that all the elements of a truly pedestrian-sensitive, mixed-use community are included in NorthWest Crossing.

In the summer of 2001 the City of Bend adopted the NorthWest Crossing overlay zone, implementing the master plan (amended in 2005 and 2013). The purpose of this document is to establish building prototypes to guide the development according to the master plan and zoning for NorthWest Crossing. The information in this Prototype Handbook is intended to help you, your designer, architect, builder, contractor and/or landscape professional to understand the guidelines regarding the allowable uses and site restrictions in NorthWest Crossing.

Rules & Design Guidelines have also been developed. The Construction Approval section describes the application and approval process. The Architectural Standards section describes allowable standards to be used in the design and construction process. The NorthWest Crossing

Architectural Review Committee (ARC) wants to ensure that the design review and approval process is administered fairly and effectively for the benefit of individual property owners and for all NorthWest Crossing residents. Exceptions to the Prototype Handbook and Rules & Design Guidelines will be considered based on architectural merit as determined by the ARC. West Bend Property Company LLC reserves the right to change the information contained within this document as it deems necessary.

We urge you to contact the ARC prior to filing any application to be sure you have the most current application form, Prototype Handbook and a current set of Rules & Design Guidelines. Please stop by our office, call, write, or e-mail with your comments or questions:

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(541) 312-6473
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Vision of the Development and Design Quality

NorthWest Crossing is a mixed-use community located on the west side of Bend, Oregon. Its comprehensive master plan provides residents an opportunity for a wholesome and enriched quality of life.

NorthWest Crossing is located a short distance from the heart of downtown Bend in an environment of pine and juniper forest, open and green spaces, with natural rock outcroppings and ridgelines.

NorthWest Crossing’s careful design encourages pedestrian and bicycle travel, with the intent to reduce dependence on the automobile. Trails and pathways connect throughout. Parks and open spaces encourage interaction with nature. Natural contours have been preserved and considered in every stage of development, resulting in a community that is sensitive to, and cooperates with, the natural landscape.

NorthWest Crossing is a dynamic and diverse community. It has educational, cultural and recreational amenities as well as forest and mountain views. NorthWest Crossing captures the essence of West Bend, invoking pleasant memories of delightful older neighborhoods, while including contemporary conveniences to provide a balanced community. Architecture will vary, patterned primarily after northwest classic designs. Neighborhoods are comfortable, connected, safe and friendly. There are many options for a broad range of buyers, from single-family homes to multi-family and live/work units. Retail opportunities abound to satisfy the daily needs of the community’s





residents. Many residents can walk to work, schools, offices, restaurants, and retail stores in NorthWest Crossing or to the nearby Oregon State University Cascades Campus and Central Oregon Community College.

Brooks Resources Corporation and Tennant Family Limited Partnership jointly own West Bend Property Company LLC, the developer of NorthWest Crossing. Brooks and Tennant have established histories of commitment to thoughtful and quality development in Central Oregon. They care deeply about the communities in which they live and work.

To ensure that NorthWest Crossing is developed and maintained to the highest practical aesthetic standards, West Bend Property Company LLC and the NorthWest Crossing ARC have established certain architectural Rules & Design Guidelines. All construction is subject to all codes and ordinances as adopted by the State of Oregon, Deschutes County, and the City of Bend, and all other pertinent regulations. The most stringent regulations shall apply in the event of a conflict.

NorthWest Crossing does not have a particular architectural theme. All of the buildings and landscapes within NorthWest Crossing are expected to employ high standards. High standards for design and construction will ensure architecture and landscapes that are considerate to the site and to surrounding buildings. The Rules & Design Guidelines specifically address design and architectural objectives.

Components of NorthWest Crossing

Residential Neighborhoods

NorthWest Crossing is principally a group of residential neighborhoods with a full spectrum of housing types. Most of the housing will be single-family homes on lots ranging in size from 4,000 square feet to one-half acre. There will also be a mix of duplexes, townhomes, cottages and small apartment buildings carefully located within the neighborhoods. Higher density housing, particularly apartments and townhomes, will be located near the two commercial centers or be adjacent to parks or arterial streets. Higher density housing near the commercial centers and public parks will support commercial and community activity. Public parks are a short walk away from all residences.

Commercial Centers

The project will have two distinct mixed-use commercial centers. One is at the intersection of Mt. Washington Drive and NW Crossing Drive. This center is designed to serve as a neighborhood commercial center. It has a blend of Commercial Limited and Mixed Employment zoning. This center will contain a full mix of uses including commercial/retail, office, other Mixed Employment uses and second story housing. It will provide an identity to the project because of its central location at the arterial and collector street crossroads and its main street design and mix of uses. Fronting the buildings directly onto wide sidewalks will reinforce the pedestrian scale of the commercial center. Parking will be located on the street and in common lots behind the buildings.

The second mixed-use commercial center is at the northern terminus of NW Crossing Drive adjacent to Shevlin Park Road. This center will also contain a mix of commercial and residential uses and be designed to emphasize pedestrian use. However, this center will include businesses and services more oriented to regional commercial needs.

NORTHWEST CROSSING VISION/COMPONENTS OF NORTHWEST CROSSING

Introduction

Industrial

NorthWest Crossing set aside land for industrial development.

This industrial land will be designed to serve research and development and light manufacturing businesses in a park-like setting. This industrial employment center is well located on arterial streets, near the high school, the neighborhood commercial center and a large community park.

Parks and Schools

Four parks totaling nearly 60 acres will provide recreational opportunities, maintain some of the forested areas, and provide open space within NorthWest Crossing. High Lakes Elementary School and Summit High School are located within NorthWest Crossing. The elementary school is within a residential area and the high school is near the neighborhood commercial center and industrial zoned property.

Streets and Trails

The streets in NorthWest Crossing are designed to be safe for vehicles, bicyclists and pedestrians. They are interconnected to provide multiple routes to various destinations. The two commercial activity centers, two public schools and public parks are well connected with a network of pedestrian and bicycle facilities including a multi-use trail, extra wide sidewalks and in-street bicycle lanes.

The street network is designed to calm traffic in residential areas and provide on-street parking. Streets vary in width to correspond to the level of traffic and the uses served. The narrowest streets access single-family residences in the quietest residential neighborhoods. Vehicle access for homeowners will typically be via alleys so that the streetscape will be pleasant and safer for pedestrians and bicyclists. The streetscape visual experience will be enhanced because garage doors and garbage service will be primarily at the rear of residences. A multi-use, off-street trail system will provide recreation and connections to the city and within NorthWest Crossing.

An essential feature of a pedestrian scaled collector street is on-street parking to create a buffer between traffic and sidewalks, to calm traffic in commercial and residential areas and to provide direct access to the front doors of businesses and residences. In NorthWest Crossing buildings will be oriented to collector or arterial streets. The collector street standard has been carefully designed to accommodate vehicles,

in-street bicycle lanes, parking and wide sidewalks. Homes will front on the collector streets with alley access to residential garages. Business and office uses will also front the collector streets and use on-street parking in addition to parking in the rear or at the side of the buildings.

Summary

NorthWest Crossing is planned to facilitate human social interaction by the design and execution of its infrastructure. NorthWest Crossing will be the place where the essence of West Bend is recaptured to provide a desired and valued community.

Prototype Descriptions

NorthWest Crossing consists of four prototypes, which represent the intended building character upon the desired use of the land. The first is the Town prototype, which represents buildings located closer to the core/town center of the project with the most urban character and feel. The second is the Village prototype, which represents buildings a bit farther away from the town center and acts as a transition between urban and neighborhood character. The third is the Neighborhood prototype, which represents the most residential feel and location. And last is the Edge prototype, which represents buildings in the most rural setting and on the edge of the property.

Town Prototypes

The Town prototypes are the most urban in character, the most massive and tall, and will create the most active streetscape of all the prototypes. Buildings will be mostly two stories fronting the street with a storefront character, creating a main street pedestrian-friendly environment. Buildings are generally built right up to the front lot line. Street-facing facades are tall to frame the street. Single level building facades are required to be at least 20' high facing the street. Parking is located at the rear of the lot, off of an alley, except in certain instances, as noted, where parking is allowed at the sides of buildings. Buildings may be attached. However, continuous attached building front facades should not exceed 230 feet without a pedestrian passageway for access to the rear alley and parking.

Village Prototypes

The Village prototypes are not quite as urban as the Town prototypes; although they may be as massive and tall as the Town buildings, the preferred setback is 10 feet from the front lot line (except for Village Industrial prototype, where the preferred front setback is 20 feet). Except for the Village Industrial prototype, the area within the setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and may be set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades may be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. The preferred location for parking is at the rear of the lot, off of an alley. Except for the Village Cluster prototype, buildings are generally attached, however, no fewer than two, and no more than four buildings shall be attached without a sideyard or passageway. Passageways should be designed for pedestrian access to the alley and mid-block parking.

Neighborhood Prototypes

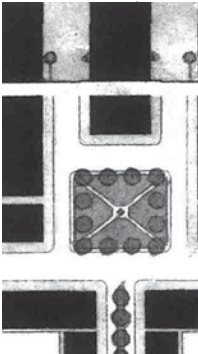
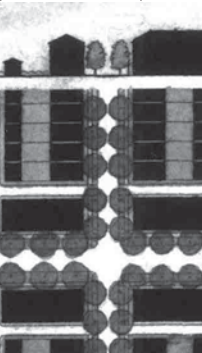
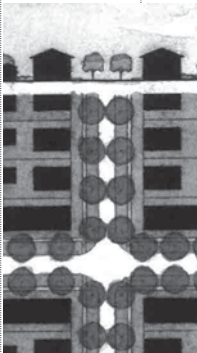
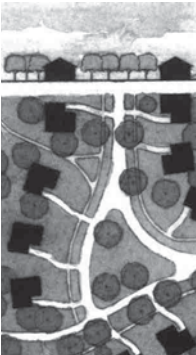
The Neighborhood prototypes are the most residential in character of all the prototypes, and are the most prevalent building type found in the NorthWest Crossing development. Buildings are detached, on lots ranging from small (4,000 to 5,999 square feet) to medium (6,000 to 7,999 square feet) to large (8,000 square feet or greater). Off-street parking is typically located at the rear of the lot, off of the alleys, in detached or attached garages. Accessory dwelling units are allowed. While lot sizes and Neighborhood prototypes are intermixed throughout much of NorthWest Crossing, subdistricts may be created through the specification of building massing guidelines. For example, in one subdistrict, buildings may appear more vertical and massive against the street, with ridges running perpendicular to the street, so that gable ends face the street or, if parallel, prominent gable dormers face the street. In other subdistricts, by contrast, buildings may be horizontal and less massive at the street facing facade of large lots, with ridges running parallel to the street, and shed dormers or small gable dormers facing the street. In addition, subdistricts may be created through the specification of more restrictive setbacks from the front lot line—a 15 foot setback, for instance, rather than the 10 foot setback allowed.

Edge Prototypes

The Edge prototypes are designated for placement in areas where the lots are irregular, extra deep, and/or are at the edge of natural or unbuildable areas.

ALL PROTOTYPES. GENERAL DESCRIPTION

Introduction

Town Prototype 1 Town Attached Commercial	Town Prototype 2 Town Detached Commercial	Town Prototype 3 Town Mixed-Use	Town Prototype 4 Town Apartment	Town Prototype 5 Town Townhome	Village Prototype 1 Village Mixed-Use	Village Prototype 2 Village Apartment	Village Prototype 3 Village Townhome	Village Prototype 4 Village Duplex	Village Prototype 5 Village Cluster	Village Prototype 6 Village Industrial	Neighborhood Prototype 1 Neighborhood Small Lot	Neighborhood Prototype 2 Neighborhood Medium Lot	Neighborhood Prototype 3 Neighborhood Large Lot	Edge Prototype 1 Edge Residential
Town Prototypes					Village Prototypes						Neighborhood Prototypes			Edge Prototypes
														

	DU/ACRE	BUILDING HEIGHT		LOT REQUIREMENTS				SETBACKS							
PROTOTYPE	DENSITY Net	Min	Max	WIDTH minimum	DEPTH typical	SIZE minimum	COVERAGE maximum	Build-to line/ frontage	Front min. (3)	Front max.	Front preferred	Side (3)	Rear	Garage rear (garage if abutting an alley) min.	Garage front min.
T-1. Town Attached Commercial	varies	20 feet	30 ft. CL 45 ft. ME (1)	varies	105 feet	NA	No max.	0 feet/75% of building frontage (6)	0 feet	5 feet (5)	0 feet	0 feet (2)	5 feet (7)	7.5 feet	NA
T-2. Town Detached Commercial	varies	20 feet	30 ft. CL 45 ft. ME (1)	varies	105 feet	NA	No max.	0 feet/75% of building frontage (6)	0 feet	10 feet	5 feet	0 feet (2)	5 feet (7)	7.5 feet	NA
T-3. Town Mixed-Use	NA	20 feet	30 feet (1)	40 feet	105 feet	4,000 sf	No max.	5 feet/75% of building frontage (6)	5 feet	10 feet	5 feet	0 feet (2)	5 feet (7)	7.5 feet	NA
T-4. Town Apartment	19	20 feet	45 feet (1)	varies	105 feet	varies	No max.	5 feet/75% of building frontage (6)	5 feet	10 feet	10 feet	5 feet (2)	5 feet (7)	7.5 feet	NA
T-5. Town Townhome	12	20 feet	35 feet (1)	24 feet	varies	2,500 sf	No max.	5 feet/75% of building frontage (6)	5 feet	10 feet	5 feet	0 feet (2)	5 feet (7)	7.5 feet	NA
V-1. Village Mixed-Use	NA	NA	30 feet (1)	24 feet	105 feet	4,000	No max.	NA	5 feet	20 feet	10 feet	10 feet (2)	5 feet (7)	7.5 feet	NA
V-2. Village Apartment	19	NA	30 - 45 feet (1)	varies	105 feet	varies	60%	NA	5 feet (12), (13)	No max.	10 feet	10 feet (2)	5 feet (7)	7.5 feet	NA
V-3. Village Townhome	12	NA	35 feet (1)	24 feet	105 feet	2,500 sf	No max.	NA	5 feet	20 feet	10 feet	0 feet (2)	5 feet (7)	7.5 feet	NA
V-4. Village Duplex	NA	NA	30 feet (1)	77 feet	105 feet	8,000 sf	35%	NA	10 feet	20 feet	10 feet	5 feet (2)	5 feet	5 feet	10 feet behind front of house (4)
V-5. Village Cluster	12	NA	30 feet (1)	varies	varies	2,000 sf	No max.	NA	10 feet	20 feet	10 feet	5 feet	5 feet	5 feet	NA
V-6. Village Industrial	NA	NA	45 feet (1)	150 feet	150-400 feet	.5 acre	50%	NA	20 feet	No max.	20 feet	15 feet	5 feet	15 feet	NA

ALL PROTOTYPES. SUMMARY TABLE

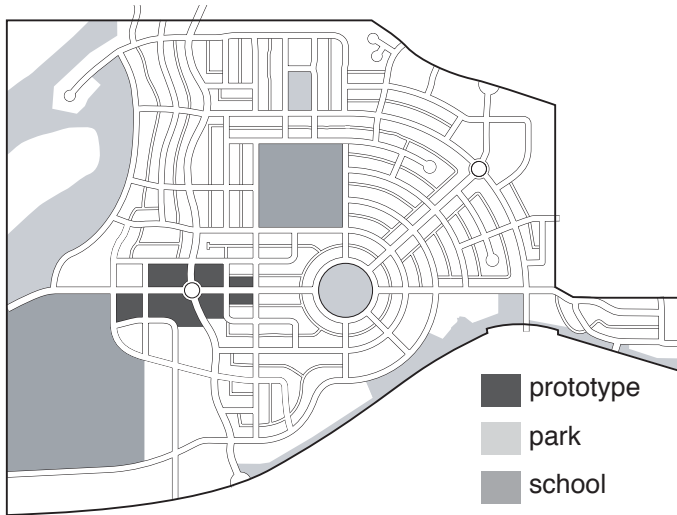
Introduction

	DU/ACRE	BUILDING HEIGHT		LOT REQUIREMENTS				SETBACKS							
PROTOTYPE	DENSITY Net	Min	Max	WIDTH typical	DEPTH typical	SIZE minimum	COVERAGE maximum	Build-to line/ frontage	Front min. (3)	Front max.	Front preferred	Side (3)	Rear	Garage rear (garage if abutting an alley)	Garage front min.
N-1. Neighborhood Small Lot	7.3 max.	NA	30 feet	40 feet	105 feet	4,000 sf	50% (8)	NA	10 feet	20 feet (9), (10)	10 feet	5 feet	5 feet (7)	5 feet (7)	NA
N-2. Neighborhood Medium Lot	7.3 max.	NA	30 feet	57 feet	105 feet	6,000 sf	35% (8)	NA	10 feet	20 feet (9), (10)	10 feet	5 feet	5 feet (7)	5 feet (7)	32 feet behind front of house (13)
N-3. Neighborhood Large Lot	7.3 max.	NA	30 feet	77 feet	105 feet	7,875	35% (8)	NA	10 feet	no max	no preferred	5 feet	5 feet (7)	5 feet (7)	32 feet behind front of house (13)
E-1. Edge Residential	2 min.	NA	30 feet	77 feet	varies	8,000	35% (8)	NA	10 feet	no max	10 feet	5 feet	5 feet	7.5 feet	32 feet behind front of house (13)

- (1) Building height may be higher with ARC approval and City of Bend conditional use permit.
- (2) If not a zero lot line, side setback is 5 feet.
- (3) Building setback must meet City of Bend's clear vision requirement.
- (4) Garages must be accessed from the alley if an alley exists.
- (5) Setbacks adjacent to a roundabout right of way shall be determined on a case-by-case basis. Buildings shall generally be close to the build-to line without limiting clear vision area.
Buildings may be set back to accommodate pedestrian amenities.
- (6) Frontage requirement: 75% of the lot frontage must have a building. The building front facade must meet minimum height requirement. A minimum of 60% of the building(s) must be at the build-to line.
No part of the building front may exceed the maximum front setback. Only 40% of the required building frontage may meet maximum front setbacks. The ARC may consider exceptions on a case-by-case basis.
- (7) When abutting a residential lot, 5 feet plus 1 foot for each foot by which the building exceeds 15 feet.
- (8) The maximum building square footage, including garage, shall not exceed 48% of lot square footage (see N-1 specifications). This massing restriction is calculated based upon the total square feet of the building including areas with heights of 5 feet or higher for all lots platted after 2005.
- (9) For lots fronting Mt. Washington Drive, the front maximum setback in excess of 20 feet will be considered by the ARC on an individual design review basis.
- (10) Exceptions will be considered by the ARC on an individual design review basis.
- (11) 5 feet front setback in Residential Mixed-Use Overlay District.
- (12) 10 feet front setback in Multi-Family Overlay District.
- (13) 10 feet from front face of home or perpendicular to street; both requiring architectural enhancements to de-emphasize garage.

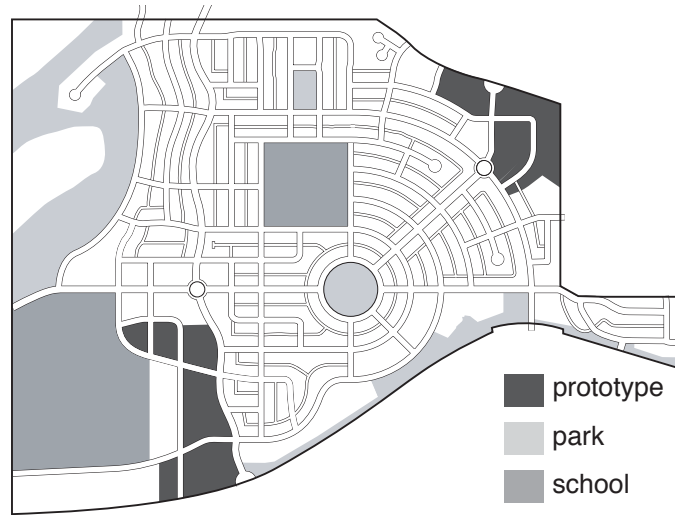
ALL PROTOTYPES. SUMMARY TABLE

Introduction



T.1 Town Attached Commercial

Description: This building prototype is the most urban in character, the most massive and tall, and will create the most active streetscape of all the prototypes. Buildings will be mostly two stories fronting the street with a storefront character, creating a main street pedestrian-friendly environment. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 0 feet. Street-facing facades are tall to frame the street. Single level buildings are required to be at least 20’ high along their street-facing facade. Parking is located at the rear of the lot.

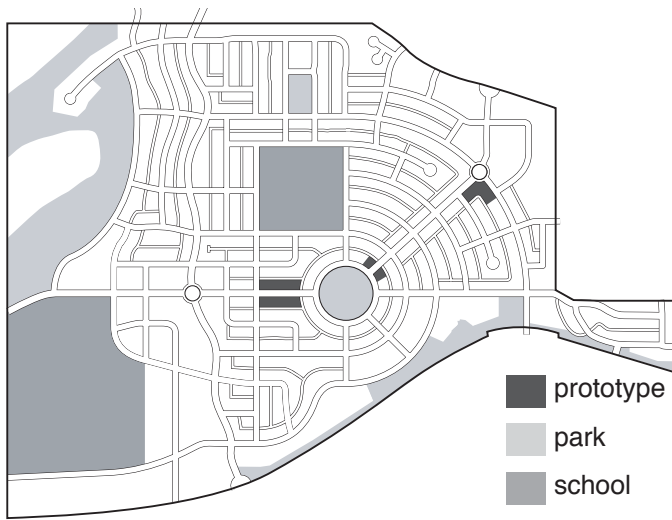


T.2 Town Detached Commercial

Description: This building prototype is intended to accommodate more surface parking in an urban setting. Buildings will be mostly two stories fronting the street with a storefront character, even though the storefront may accommodate a non-retail office. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the front setback. The preferred setback from the front lot line is 5 feet. Street-facing facades are tall to frame the street. Single level buildings are required to be at least 20’ high along their street-facing facade. Parking is allowed to the sides of buildings, although the preferred location for parking is at the rear of the lot.

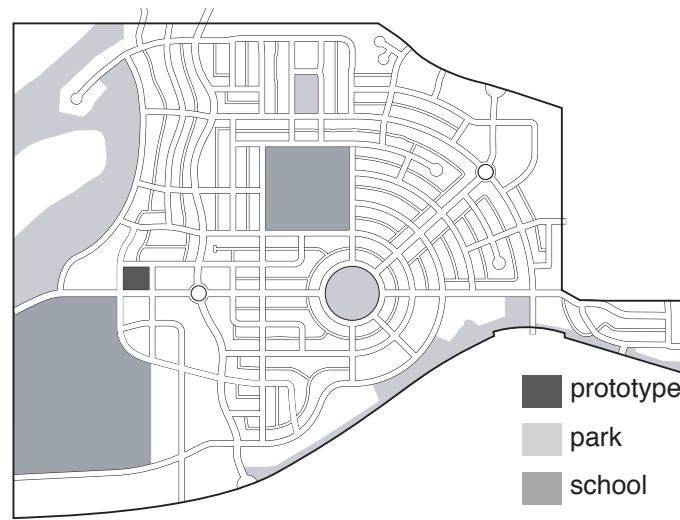
LOCATION SUMMARY. ALL TOWN PROTOTYPES

Town Prototypes



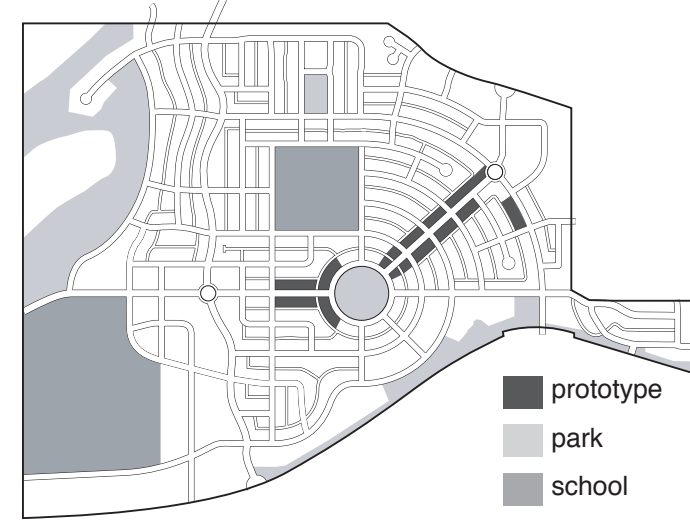
T.3 Town Mixed-Use

Description: This building prototype is intended for small scale community commercial buildings or live/work buildings with combined residential and commercial or professional office uses. The live/work buildings could be townhomes or apartment buildings with business at the ground floor and residential uses above or behind the business use. Buildings will be mostly two stories fronting the street with a storefront character, even though the storefront may accommodate a non-retail office, home office or residential use. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 5 feet. Street-facing facades are tall to frame the street. Single level buildings are required to be at least 20’ high along their street-facing facade. All parking is located at the rear of the lot, off of an alley.



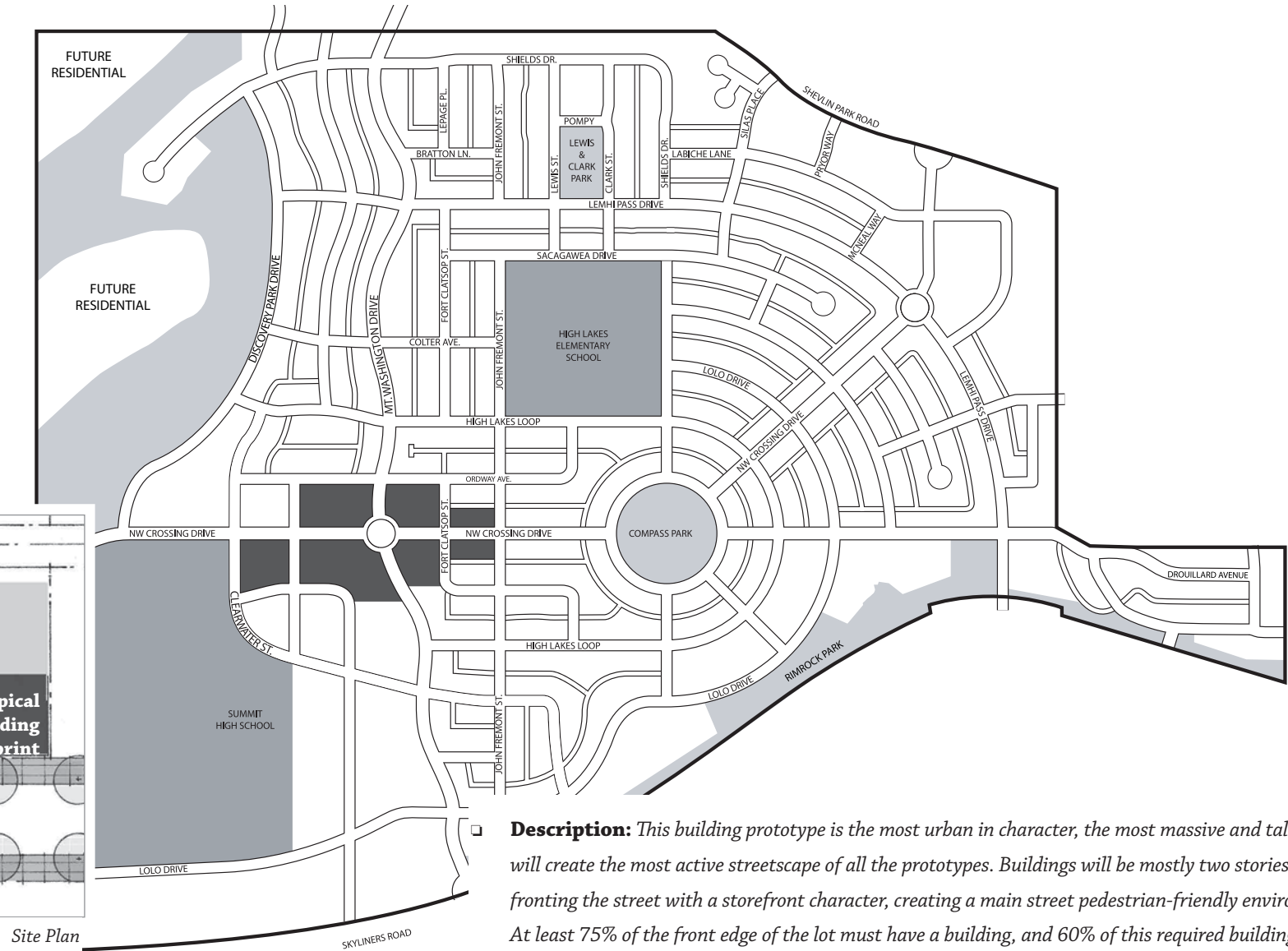
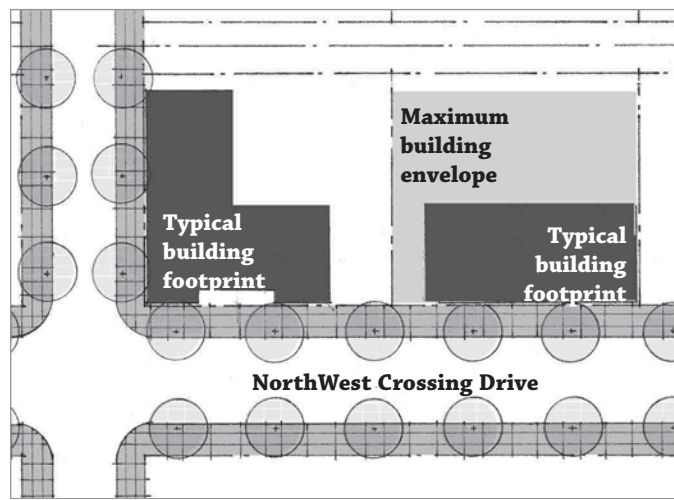
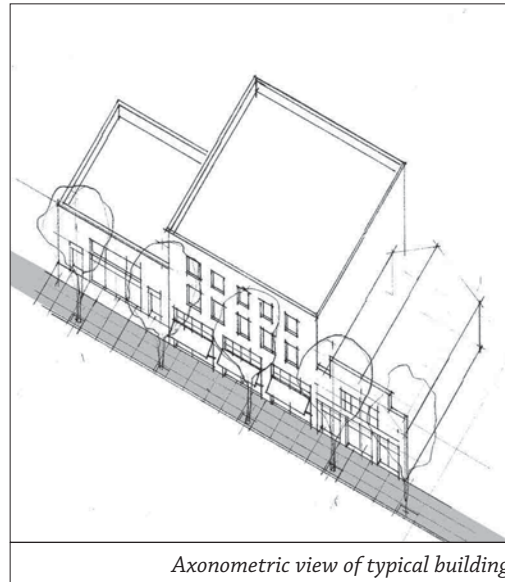
T.4 Town Apartment

Description: This building prototype is intended to accommodate residential multiple-family uses in an urban setting. Buildings will be mostly two stories fronting the street. Instead of storefronts, offices or retail uses, gracious apartment lobbies and apartments will be located at the ground level. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 10 feet. Street-facing facades are tall to frame the street. Buildings may be up to 45 feet in height. Single level buildings are required to be at least 20’ high along the street-facing facade. All parking is located at the rear of the lot, off of an alley.



T.5 Town Townhome

Description: This building prototype is intended to accommodate residential uses in a townhome building. Buildings are required to be attached, however, no fewer than four, and no more than six townhomes shall be attached without a sideyard or passageway. Buildings will be mostly two stories fronting the street. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 5 feet. Street-facing facades are tall, to frame the street. Buildings may be up to 35 feet in height. Single level buildings are required to be at least 20’ high along the street-facing facade. All parking is located at the rear of the lot, off of an alley.



- ❑ **Description:** This building prototype is the most urban in character, the most massive and tall, and will create the most active streetscape of all the prototypes. Buildings will be mostly two stories fronting the street with a storefront character, creating a main street pedestrian-friendly environment. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the "build-to line" (the front lot line). The preferred setback from the front lot line is 0 feet. Street-facing facades are tall to frame the street. Single level buildings are required to be at least 20' high along their street-facing facade. Parking is located at the rear of the lot.
- ❑ **Use:** Commercial, retail, office and other non-residential uses permitted in City of Bend Commercial Limited (CL) and Mixed Employment (ME) zones. Second floor residential uses allowed.

TOWN ATTACHED COMMERCIAL. KEY MAP

Town Prototypes

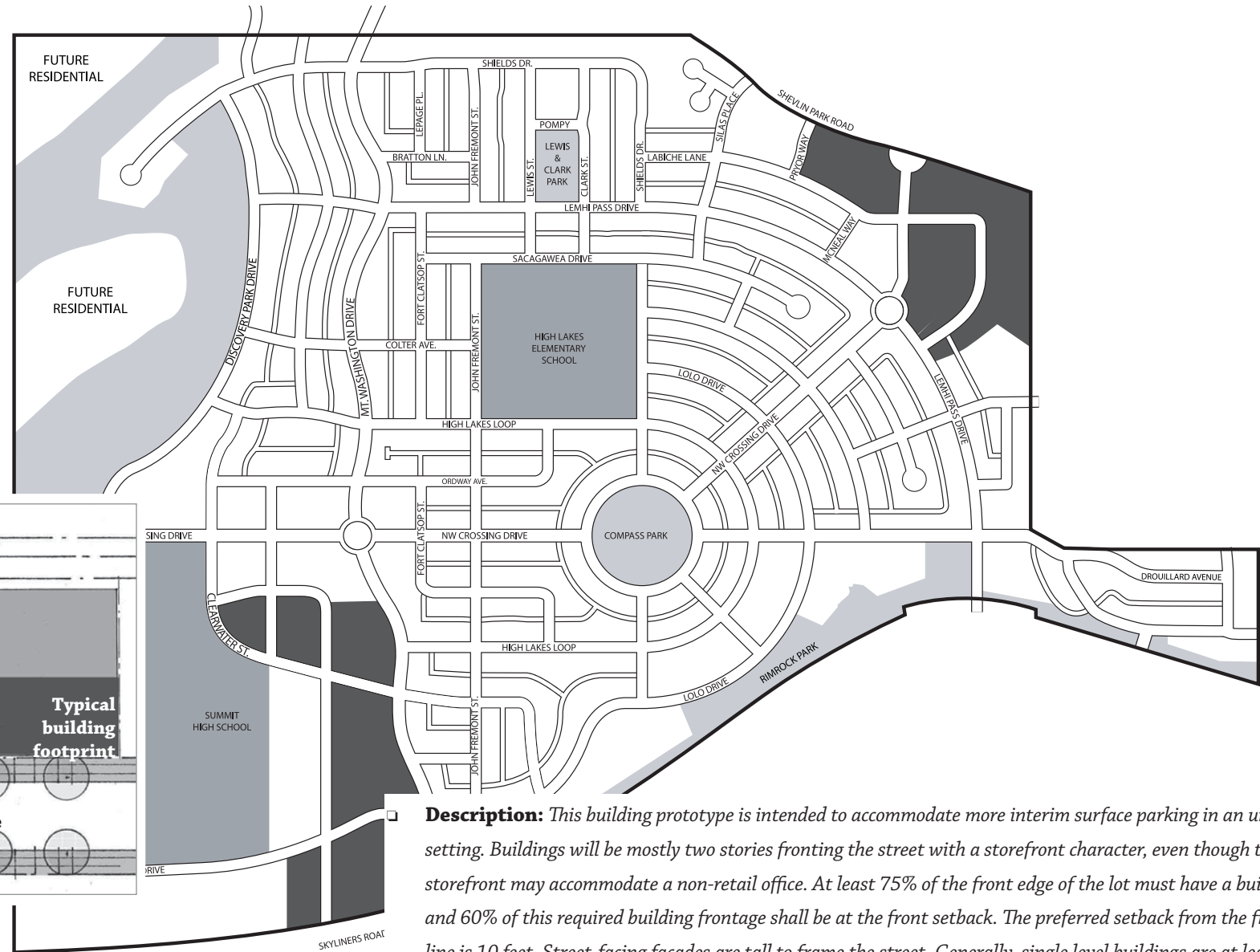
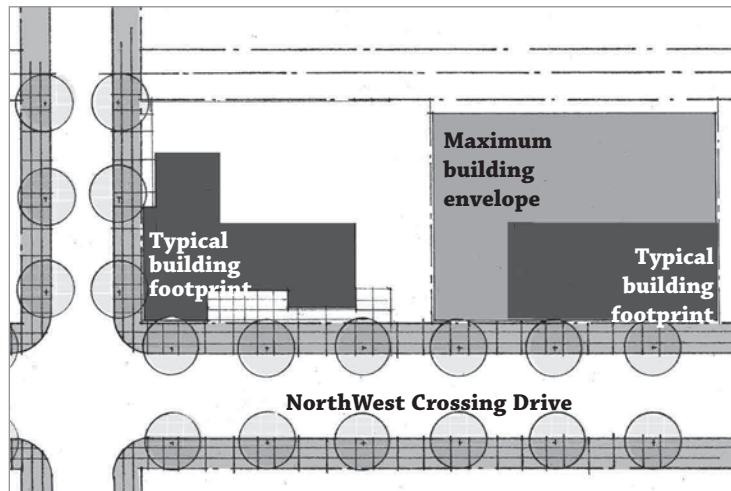
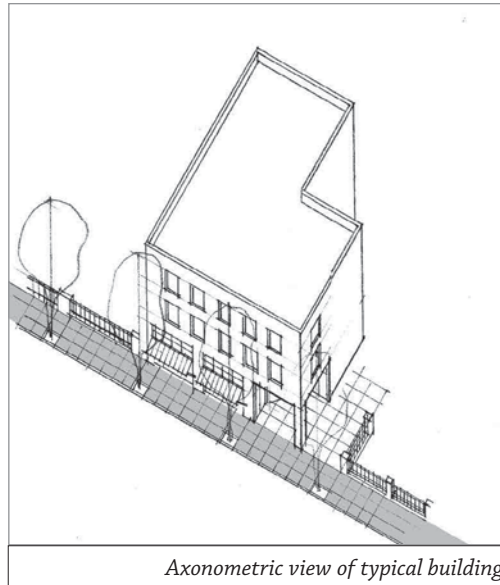
- ❑ **Dwelling units:** Permitted on second story.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 20 foot minimum height facade along the street frontage; 30 foot maximum height in the CL zone; 45 feet in the ME zone, higher with a City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Generally 230 foot deep blocks with 20 foot wide alley right of way.
- ❑ **Lot Coverage:** No maximum.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 0 feet along NW Crossing Drive. Maximum: 5 feet. Preferred: 0 feet.
 - Side setback.** If not a zero lot line, setback is 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of an alley or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** May be located in shared parking Districts.
 - On-street.** Permitted on collector and local streets.
- ❑ **Encroachments:** When front setback is 0 feet, other than awnings and blade signs, no encroachments are allowed. Where there is a front setback, however, porches, covered entries, stairs, stoops, bay windows, etc., are allowed within the setback. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Commercial Rules & Design Guidelines.
- ❑ **Lighting:** See Commercial Rules & Design Guidelines.
- ❑ **Signage:** See Commercial Rules & Design Guidelines.
- ❑ **Base Zone:** Commercial Limited (CL) Mixed Employment (ME).
- ❑ **Overlay District:** Commercial Mixed Employment Overlay District.



TOWN ATTACHED COMMERCIAL. SPECIFICATIONS

Town Prototypes

T.1



- **Description:** *This building prototype is intended to accommodate more interim surface parking in an urban setting. Buildings will be mostly two stories fronting the street with a storefront character, even though the storefront may accommodate a non-retail office. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage shall be at the front setback. The preferred setback from the front lot line is 10 feet. Street-facing facades are tall to frame the street. Generally, single level buildings are at least 20' high along their street-facing facade. Parking is allowed to the sides of buildings, although the preferred location for parking is at the rear of the lot.*
- **Use:** *Commercial, retail, office and other non-residential uses permitted in City of Bend Commercial Limited (CL) and Mixed Employment (ME) zones. Second floor residential uses allowed.*

T.2

TOWN DETACHED COMMERCIAL. KEY MAP

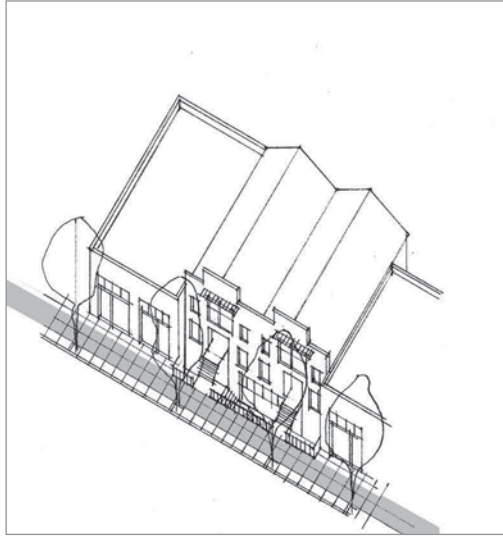
Town Prototypes

- ❑ **Dwelling units:** Permitted on second story.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 20 foot minimum height facade along the street frontage; 30 foot maximum height in the CL zone; 45 feet in the ME zone, higher with a City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Generally 230 foot deep blocks with 20 foot wide alley right of way.
- ❑ **Lot Coverage:** No maximum.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 0 feet. Maximum: 10 feet. Preferred: 5 feet.
 - Side setback.** If not a zero lot line, setback is 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of an alley or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** May be located in shared parking Districts.
 - On-street.** Permitted on collector and local streets.
- ❑ **Encroachments:** When front setback is 0 feet, other than awnings and blade signs, no encroachments are allowed. Where there is a front setback, however, porches, covered entries, stairs, stoops, bay windows, etc., are allowed within the setback. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Commercial Rules & Design Guidelines.
- ❑ **Lighting:** See Commercial Rules & Design Guidelines.
- ❑ **Signage:** See Commercial Rules & Design Guidelines.
- ❑ **Base Zone:** Commercial Limited (CL) Mixed Employment (ME).
- ❑ **Overlay District:** Commercial Mixed Employment Overlay District.

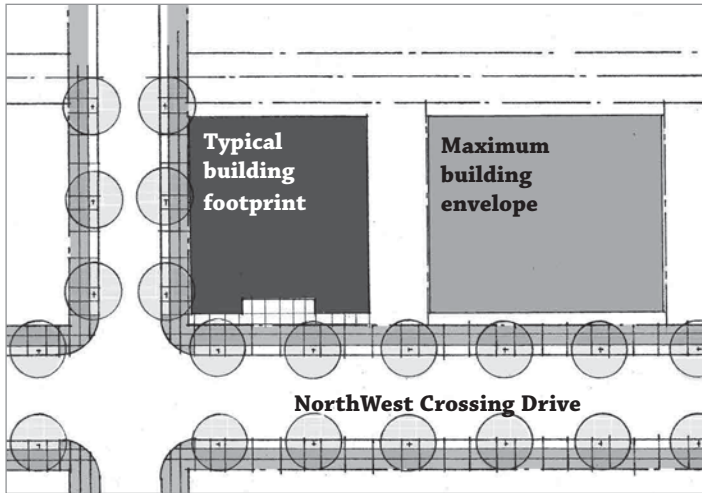


TOWN DETACHED COMMERCIAL. SPECIFICATIONS

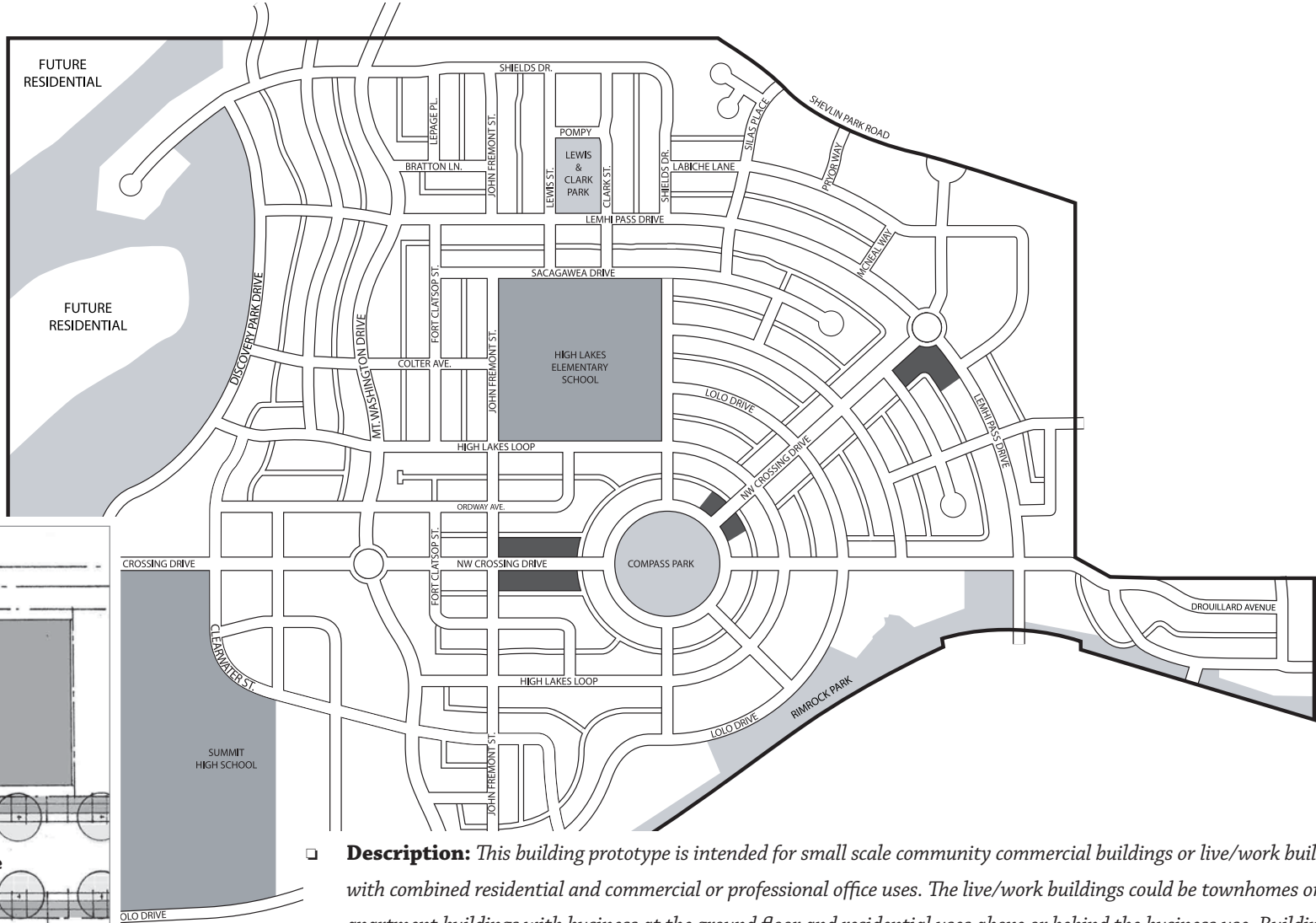
Town Prototypes



Axonometric view of typical building



Site Plan



- ❑ **Description:** This building prototype is intended for small scale community commercial buildings or live/work buildings with combined residential and commercial or professional office uses. The live/work buildings could be townhomes or apartment buildings with business at the ground floor and residential uses above or behind the business use. Buildings will be mostly two stories fronting the street with a storefront character, even though the storefront may accommodate a non-retail office, home office or residential use. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the front setback. The preferred setback from the front lot line is 5 feet. Street-facing facades are tall to frame the street. Generally, single level buildings are at least 20' high along their street-facing facade. All parking is located at the rear of the lot, off of an alley.
- ❑ **Use:** Residential, live/work and community commercial.

- ❑ **Dwelling units per acre:** 19 units per acre maximum.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 20 foot minimum facade height along the street frontage; 30 foot maximum height for community commercial, 35 feet for live/work townhome, 45 feet for live/work apartment, higher with a City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Minimum lot width for townhomes: 24 feet. Minimum lot area: 2,500 square feet. Generally, blocks are 230 foot deep with a 20 foot wide alley right of way.
- ❑ **Lot Coverage:** No maximum for townhomes, live/work townhomes or live/work apartments or multiple-family.
- ❑ **Setbacks:**

Front setback. Minimum: 5 feet. Maximum: 10 feet. Preferred: 5 feet.

Side setback. The side setback is 5 feet.

Rear setback. Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.

Rear garage setback if abutting an alley. 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**

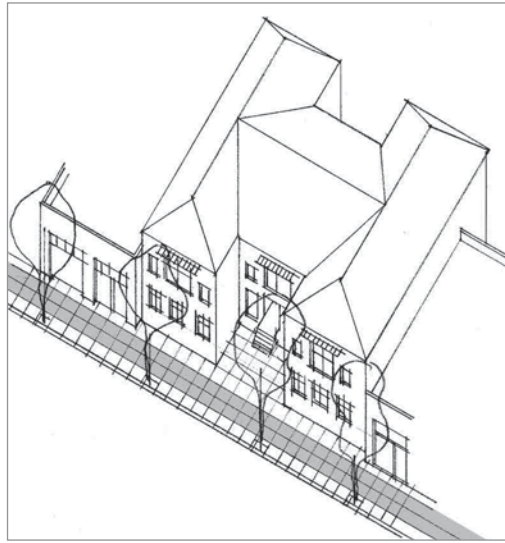
General. Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.

Live/work townhome or house. Two spaces for the residential use. Maximum of two additional spaces for each business use.

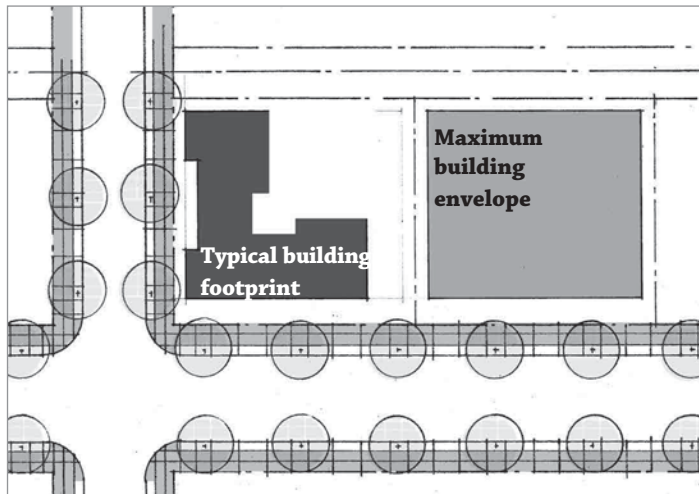
Live/work apartment. City of Bend code for both residential and business use.

Community Commercial: Minimum of one on street space for each 500 square feet of floor area. Maximum number of spaces is 5 including handicapped. Minimum of 4 covered bicycle spaces.
- ❑ **Encroachments:** For front setbacks, porches, covered entries, stairs, stoops, bay windows, etc., are allowed within the setback. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Mixed-Use Rules & Design Guidelines.
- ❑ **Lighting:** See Mixed-Use Rules & Design Guidelines.
- ❑ **Signage:** See Mixed-Use Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Mixed-Use.

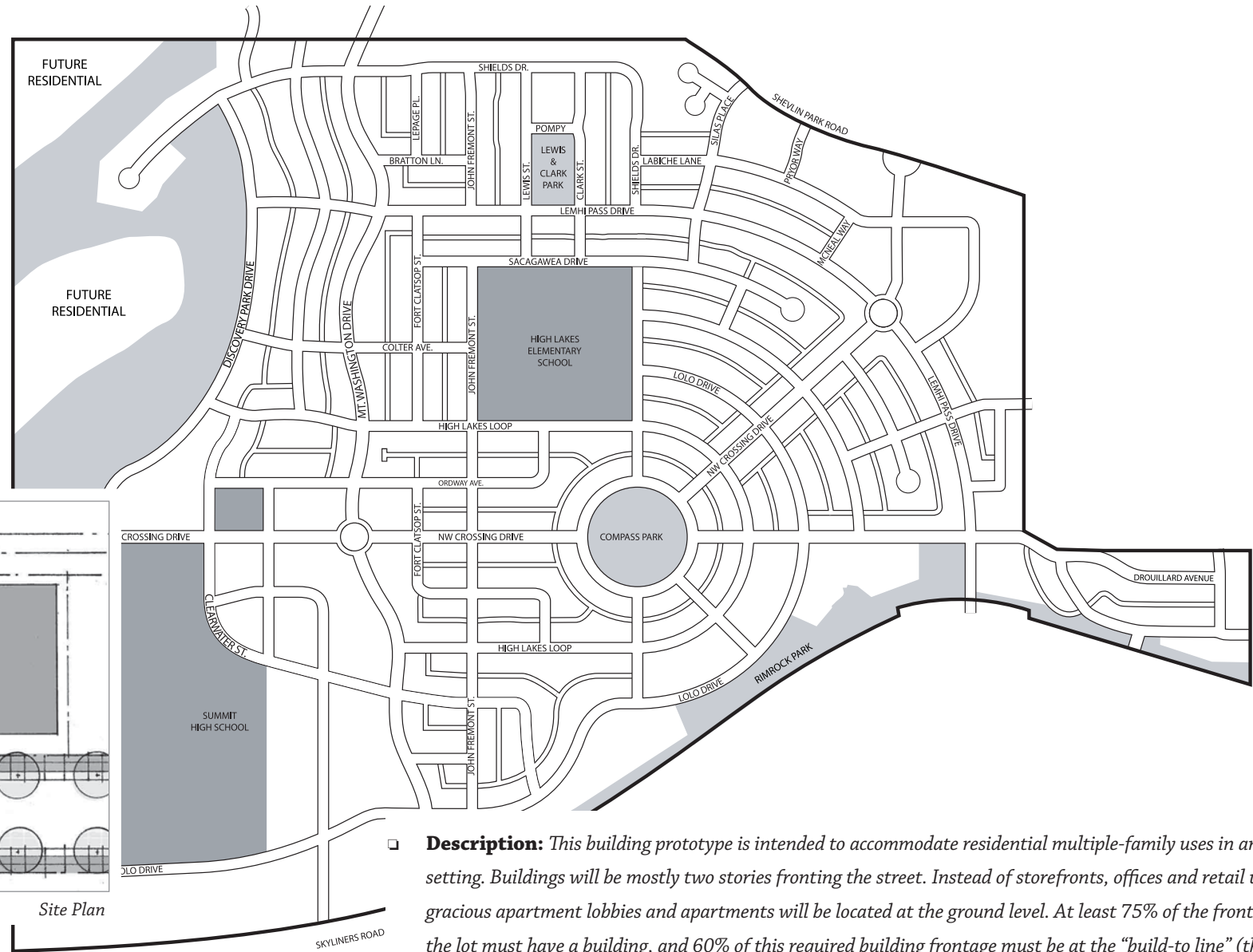




Axonometric view of typical building



Site Plan



- ❑ **Description:** This building prototype is intended to accommodate residential multiple-family uses in an urban setting. Buildings will be mostly two stories fronting the street. Instead of storefronts, offices and retail uses, gracious apartment lobbies and apartments will be located at the ground level. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 10 feet. Street-facing facades are tall to frame the street. Buildings may be up to 45 feet in height. Generally, single level buildings are at least 20’ high along the street-facing facade. All parking is located at the rear of the lot, off of an alley.
- ❑ **Use:** Residential Multiple-Family.

TOWN APARTMENT. KEY MAP

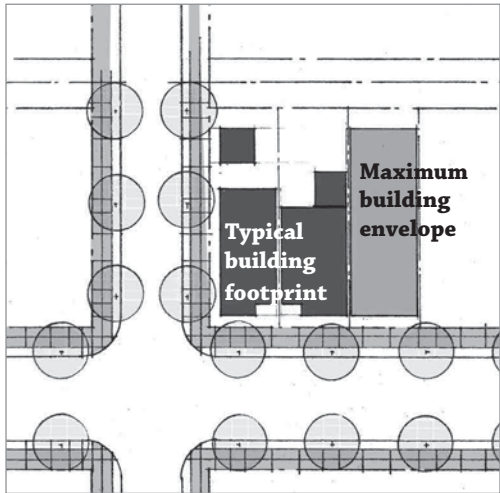
Town Prototypes

- ❑ **Dwelling units per acre:** 19 units per acre, unless up-zoned to RH densities.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 20 foot minimum height facade along the street frontage; 45 foot maximum height, higher with a City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Generally 230 foot deep blocks with 20 foot wide alley right of way.
- ❑ **Lot Coverage:** No maximum.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 5 feet in Mixed-Use Overlay District and 10 feet in Multiple-family Overlay District. Maximum: 10 feet. Preferred: 10 feet.
 - Side setback.** The side setback is 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee. Number of spaces as per City of Bend code for multiple-family housing.
- ❑ **Encroachments:** For front setbacks, porches, covered entries, stairs, stoops, bay windows, etc., are allowed within the setback. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Lighting:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Signage:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Mixed-Use and Residential Multiple-Family.





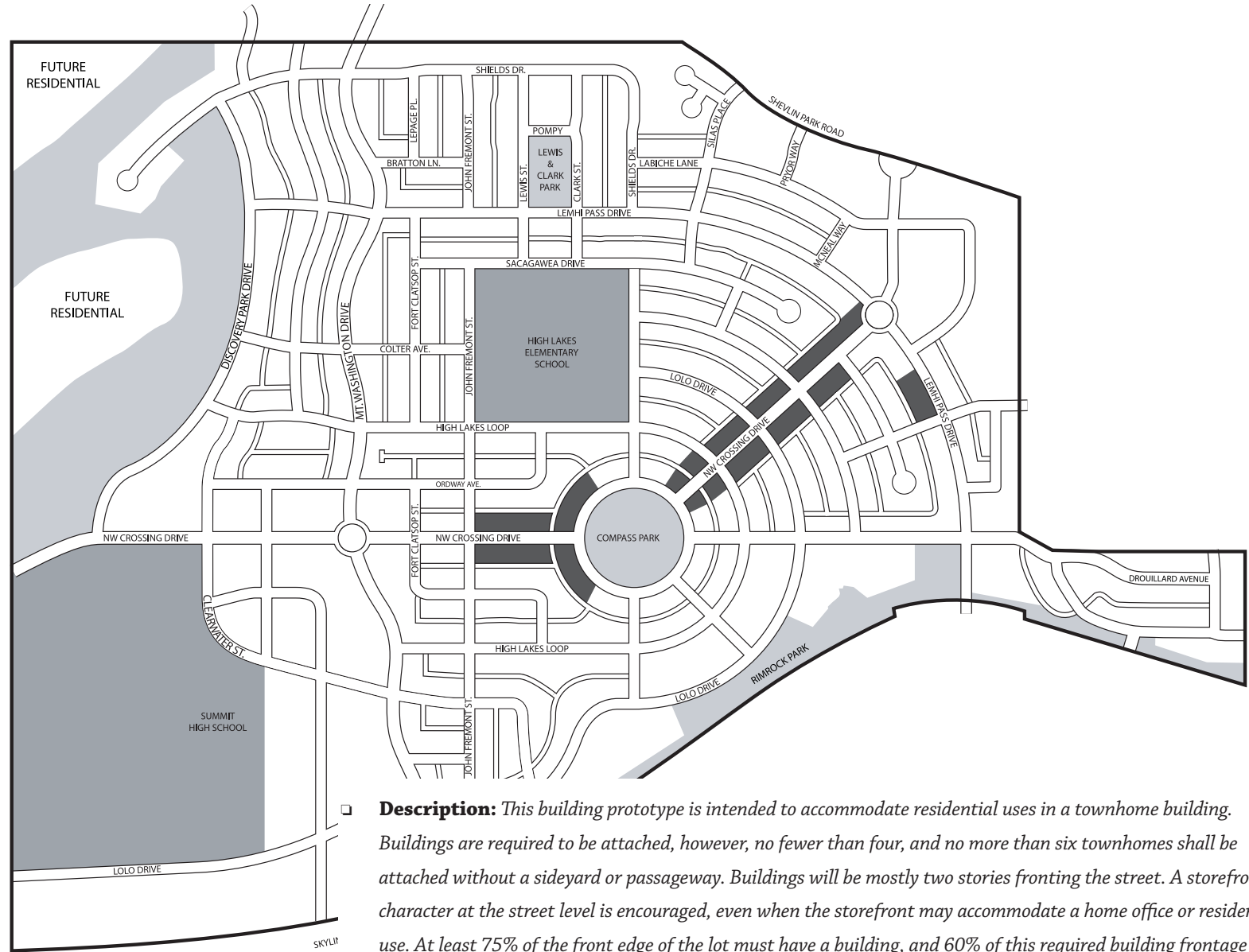
Axonometric view of typical building



Site Plan

T.5

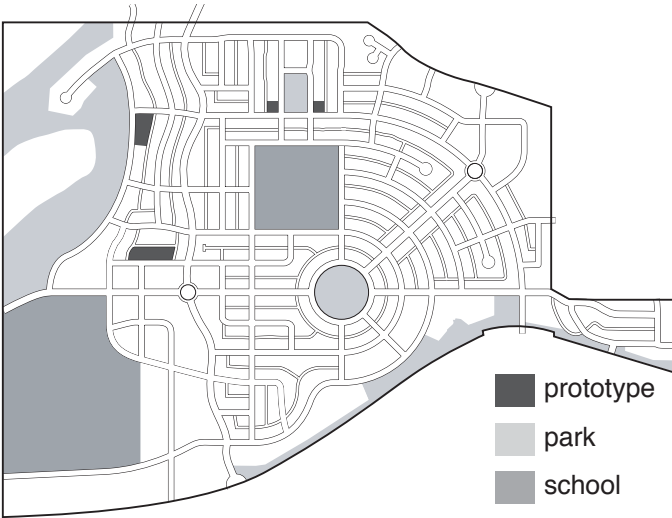
TOWN TOWNHOME. KEY MAP



- **Description:** This building prototype is intended to accommodate residential uses in a townhome building. Buildings are required to be attached, however, no fewer than four, and no more than six townhomes shall be attached without a sideyard or passageway. Buildings will be mostly two stories fronting the street. A storefront character at the street level is encouraged, even when the storefront may accommodate a home office or residential use. At least 75% of the front edge of the lot must have a building, and 60% of this required building frontage must be at the “build-to line” (the front lot line). The preferred setback from the front lot line is 5 feet. Street-facing facades are tall to frame the street. Buildings may be up to 35 feet in height. Generally, single level buildings are at least 20’ high along the street-facing facade. All parking is located at the rear of the lot, off of an alley.
- **Use:** Attached townhome residential.

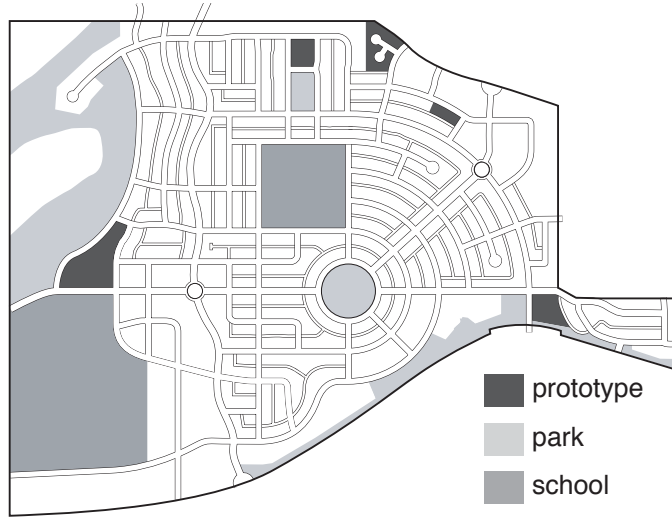
- ❑ **Dwelling units per acre:** 12 units per acre.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 20 foot minimum height facade along the street frontage; 35 foot maximum height, higher with a City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Minimum lot width for townhomes: 24 feet. Generally, blocks are 230 foot deep with a 20 foot wide alley right of way.
- ❑ **Lot Coverage:** No maximum for townhomes.
- ❑ **Setbacks:** As per City of Bend for RM zone.
 - Front setback.** Minimum: 5 feet. Maximum: 10 feet. Preferred: 5 feet.
 - Side setback.** If not a zero lot line, setback is 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off street spaces.** Two spaces per dwelling unit.
- ❑ **Encroachments:** For front setbacks, porches, covered entries, stairs, stoops, bay windows, etc., are allowed within the setback. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Lighting:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Signage:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Mixed-Use and Residential Townhome.





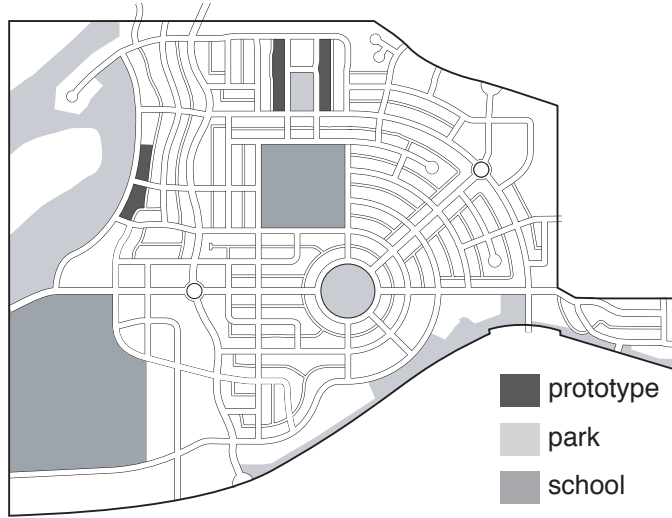
V.1 Village Mixed-Use

Description: This building prototype is intended to accommodate community commercial, live/work and residential uses in a village setting. Small scale community-serving commercial uses are encouraged at the ground level. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Accessory dwelling units are allowed. Non-residential parking is allowed to the sides of buildings, although the preferred location for all parking, residential and non-residential, is at the rear of the lot, off of an alley.



V.2 Village Apartment

Description: This building prototype is intended to accommodate residential multiple-family uses in an village setting, in the form of multiple buildings arranged around a shared landscaped green or courtyard; a building type commonly called a garden apartment. The preferred setback from the front lot line is 10 feet, and buildings may cover up to 60% of the lot. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley.

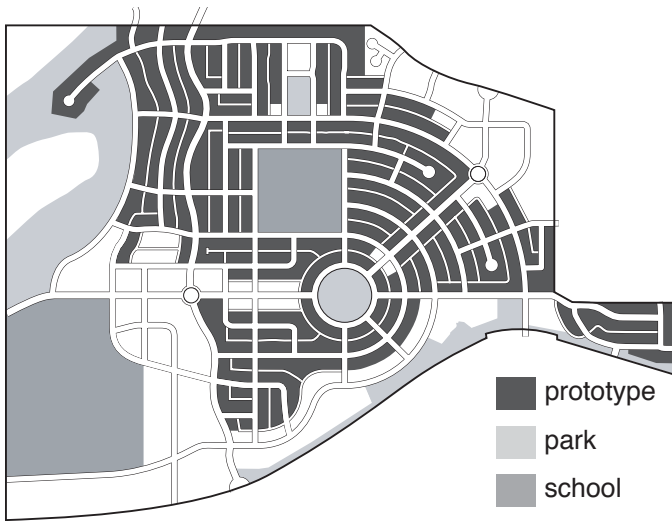


V.3 Village Townhome

Description: This building prototype is intended to accommodate residential uses in a townhome building. Buildings are required to be attached, however, no fewer than two, and no more than four townhomes shall be attached without a sideyard or passageway. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley.

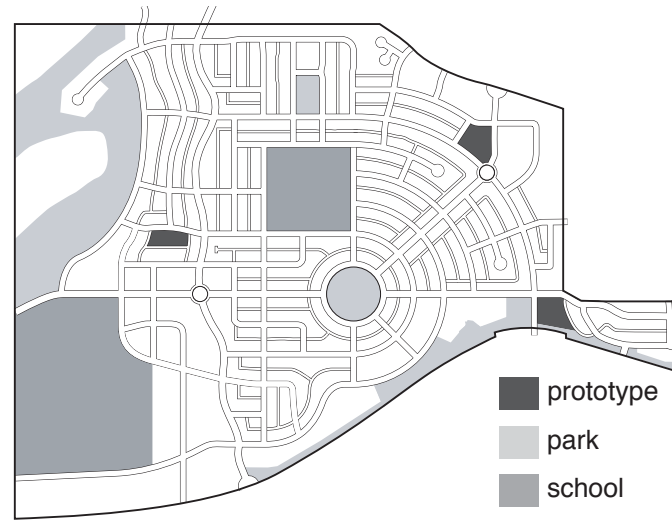
LOCATION SUMMARY. ALL VILLAGE PROTOTYPES

Village Prototypes



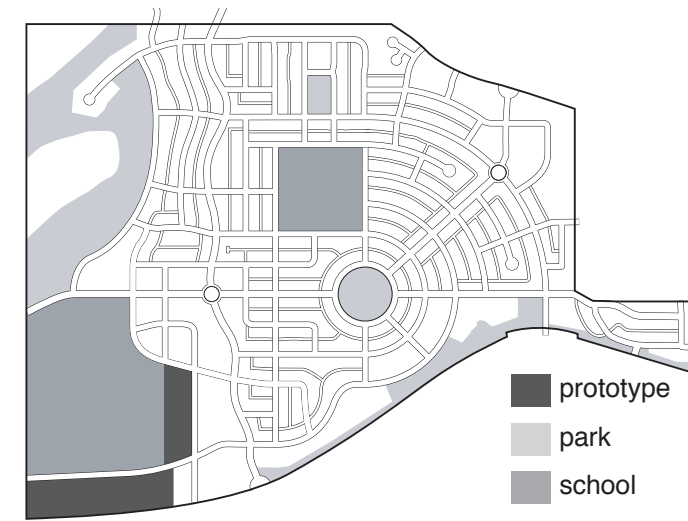
V.4 Village Duplex

Description: This prototype is ideal for location at the corners of blocks, with front doors facing each of the streets, but is not limited to corner locations. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and may be set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a detached single-family dwelling character. The Village Duplex prototype is intended to look like a detached single dwelling, even while it provides two dwelling units, and is intended to provide a graceful transition between the more massive, attached two and four-townhomes and the Neighborhood prototypes. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley. This prototype applies to only lots platted as duplex lots.



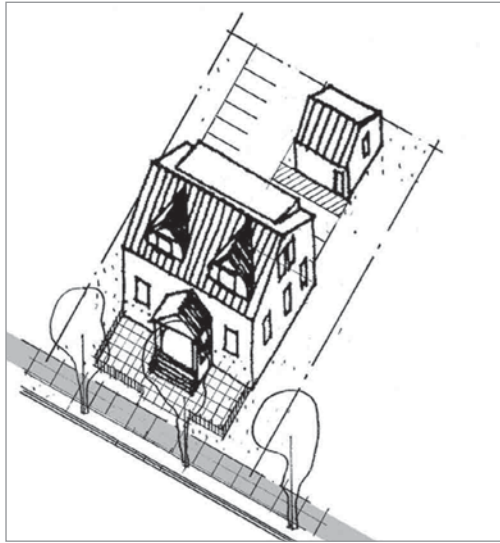
V.5 Village Cluster

Description: The Village Cluster prototype is intended to provide modestly sized detached single dwellings clustered, courtyard-style, around shared “greens.” Ancillary structures for storage and shared workshop space are allowed. Parking may be provided in shared lots at the edge of the open space, in addition to garages attached to each dwelling unit. Each cottage is designed with a porch and a small private outdoor space, in addition to the shared open space. Overall, this prototype is intended to provide modest homes, but without sacrificing the outdoor area and storage/work area that homeowners expect, and that typically represent an advancement over apartment living. This prototype is designated for areas where block size and configuration is not the typical 230’ deep block with a 20’ alley, and more unusual building arrangements are necessary. Because the open space, driveways and parking areas are shared. These buildings may be developed as condominiums.

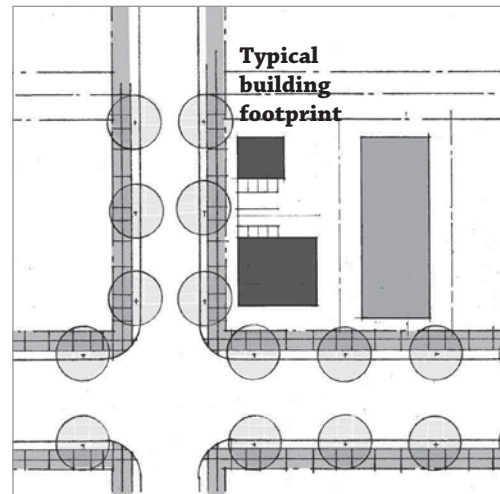


V.6 Village Industrial

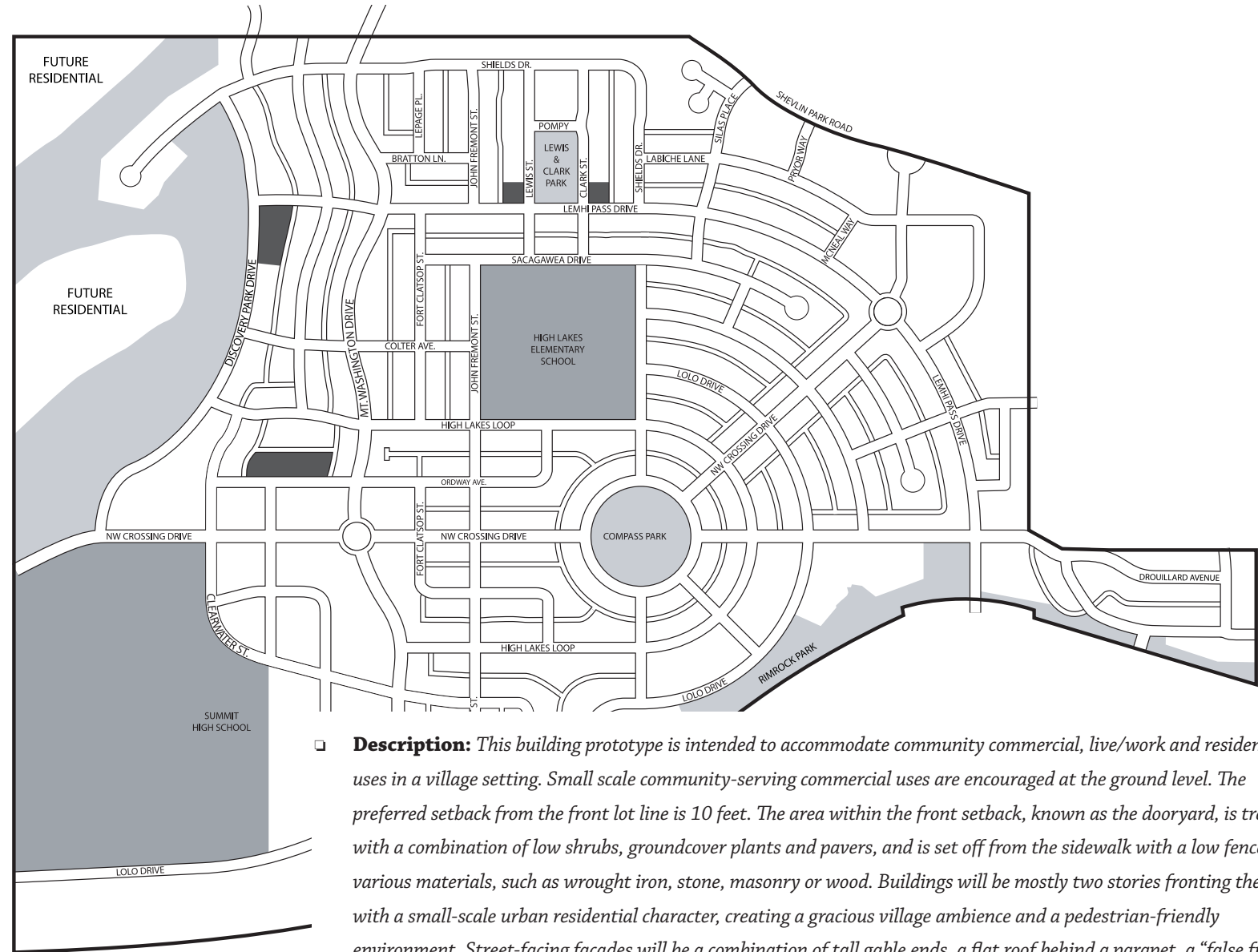
Description: The Village Industrial Prototype is designed for buildings up to two stories in an office park environment. The preferred setback from the front lot line is 20 feet. Parking is allowed to the sides of buildings, although the preferred location for parking is at the rear of the lot, off of an alley.



Axonometric view of typical building



Site Plan



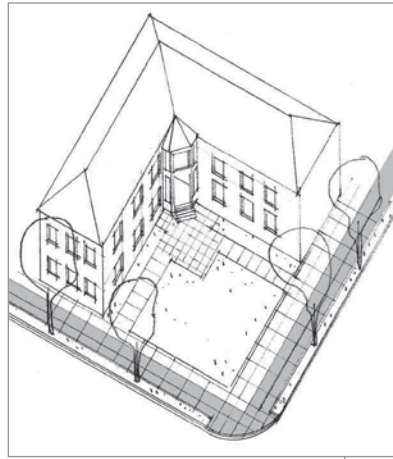
- ❑ **Description:** This building prototype is intended to accommodate community commercial, live/work and residential uses in a village setting. Small scale community-serving commercial uses are encouraged at the ground level. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Accessory dwelling units are allowed. Non-residential parking is allowed to the sides of buildings, although the preferred location for all parking, residential and non-residential, is at the rear of the lot, off of an alley.
- ❑ **Use:** Residential, live/work townhomes and apartments, and community commercial.

VILLAGE MIXED-USE. KEY MAP

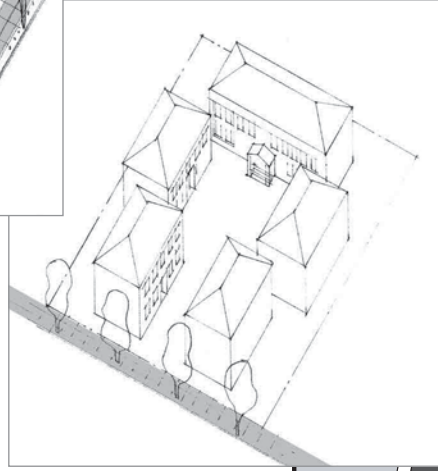
Village Prototypes

- ❑ **Dwelling units per acre:** 19 units per acre.
- ❑ **Accessory dwelling unit:** Allowed.
- ❑ **Height:** 30 foot maximum height for community commercial, 35 feet for live/work townhome, 45 feet for live/work apartment, higher with City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Minimum lot width for townhomes: 24 feet. Minimum 4,000 square feet for live/work house and community commercial, 2,500 square feet for townhomes. Generally, blocks are 230 feet deep with a 20 foot wide alley Right of Way.
- ❑ **Lot Coverage.** No maximum for townhomes, live/work townhomes, live/work apartment or multiple-family.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 5 feet. Maximum: 20 feet. Preferred: 10 feet.
 - Side setback.** 10 feet, except when abutting a residential zone, then the side setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street:**
 - Live/work townhome or house.** Two spaces for the residential use. Maximum of two additional spaces for each business use.
 - Live/work apartment.** City of Bend code for both residential and business use.
 - Community Commercial.** Minimum of one of street space for each 500 square feet of floor area. Maximum number of spaces is 5 including handicapped. Minimum of 4 covered bicycle spaces.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, stoops, bay windows, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Mixed-Use Rules & Design Guidelines.
- ❑ **Lighting:** See Mixed-Use Rules & Design Guidelines.
- ❑ **Signage:** See Mixed-Use Signage Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Mixed-Use and Residential Multiple-Family.

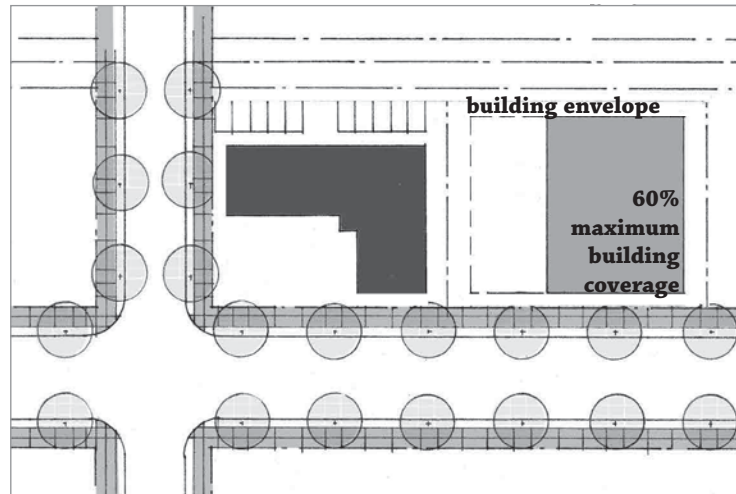




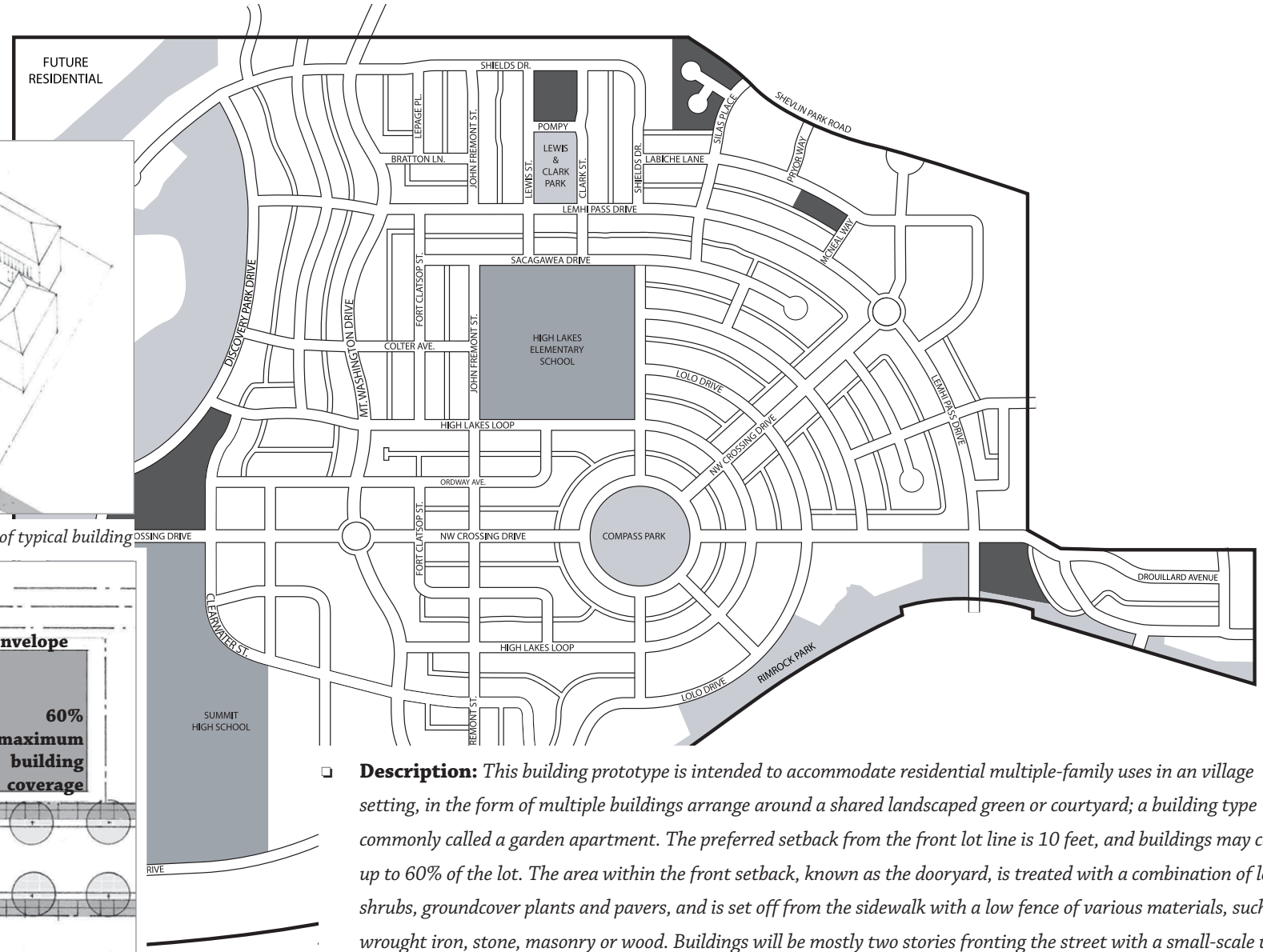
Axonometric view of typical building



Axonometric view of typical building



Site Plan



- **Description:** This building prototype is intended to accommodate residential multiple-family uses in a village setting, in the form of multiple buildings arranged around a shared landscaped green or courtyard; a building type commonly called a garden apartment. The preferred setback from the front lot line is 10 feet, and buildings may cover up to 60% of the lot. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley.

- **Use:** Residential Multiple-Family.

VILLAGE APARTMENT. KEY MAP

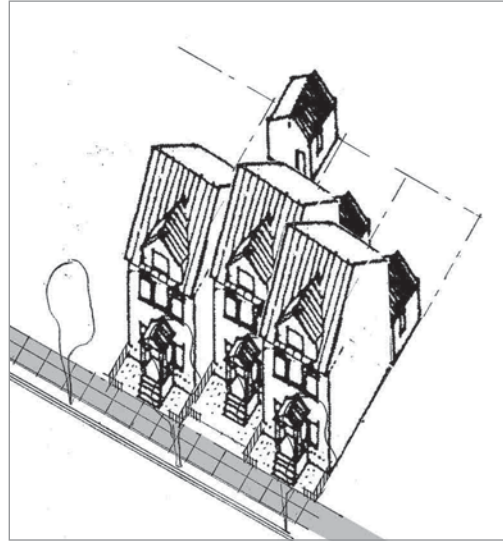
Village Prototypes

- ❑ **Dwelling units per acre:** 19 units per acre or 42 units per acre for RH zoned land.
- ❑ **Accessory dwelling unit:** Not allowed
- ❑ **Height:** 45 feet, higher with City of Bend conditional use permit.
- ❑ **Lot size:** Varies. Minimum City of Bend RH zone 2,500 square feet for first unit plus 1000 square feet for each additional unit. Generally, blocks are 230 feet deep with a 20 foot wide alley right of way.
- ❑ **Lot Coverage.** 60%.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 5 feet in Mixed-Use Overlay District and 10 feet in Multiple-family Overlay District. Maximum: no maximum. Preferred: 10 feet.
 - Side.** 10 feet, except when abutting a residential zone, then the side setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** As per City of Bend code for Multiple-family housing.
 - On-street.** Permitted on collector and local streets
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, stoops, bay windows, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Lighting:** See Multiple-Family Rules & Design Guidelines.
- ❑ **Signage:** Identification sign(s) - ground or wall mounted, not exceeding 24 square feet, approved by ARC.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Mixed-Use and Multiple-Family.

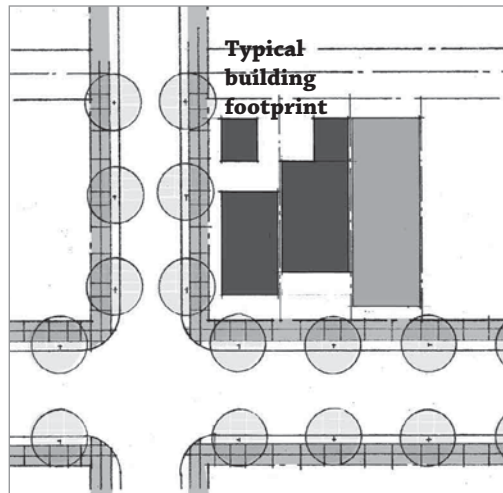


VILLAGE APARTMENT. SPECIFICATIONS

Village Prototypes



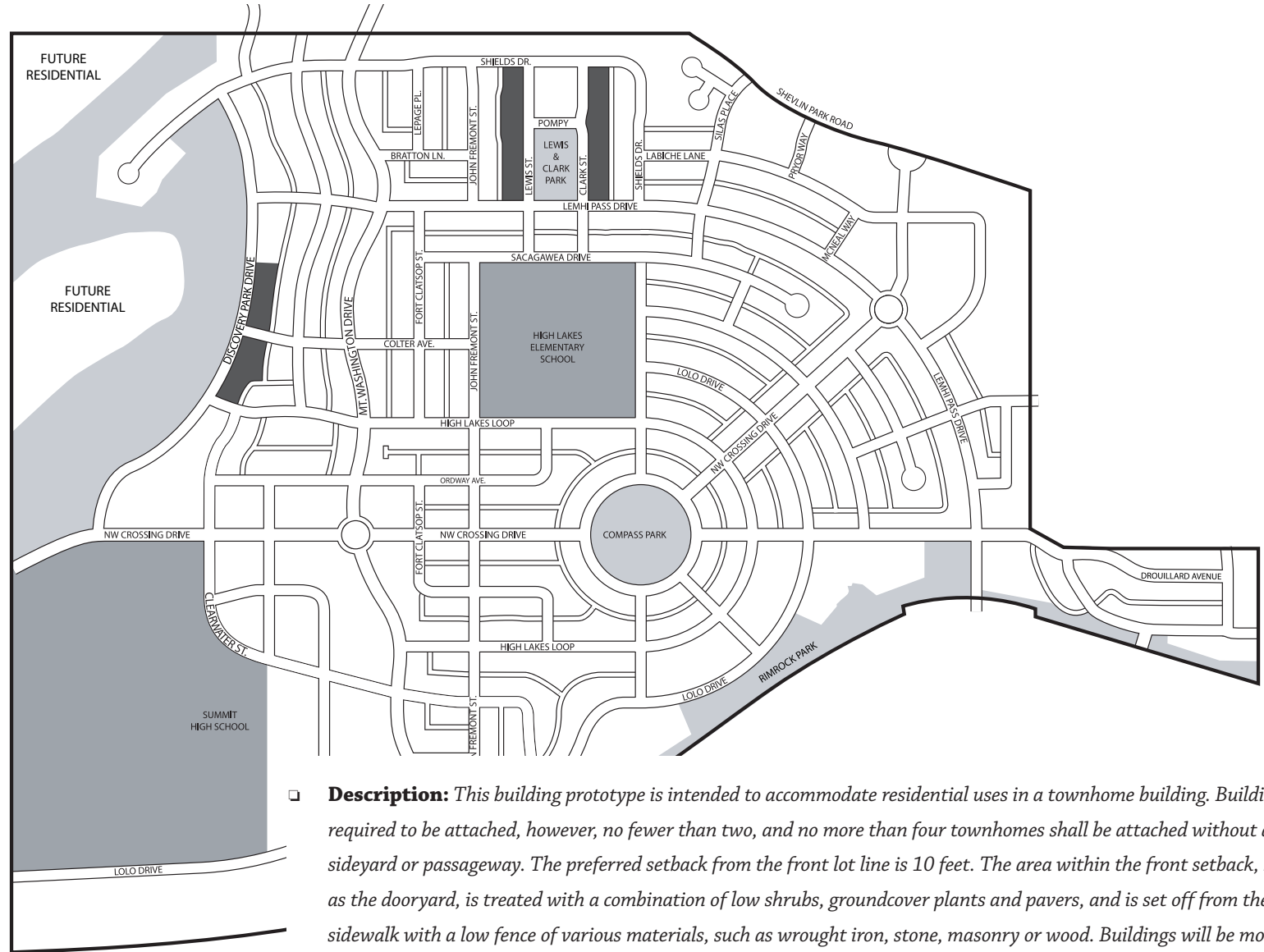
Axonometric view of typical building



Site Plan

VILLAGE TOWNHOME. KEY MAP

Village Prototypes



- ❑ **Description:** This building prototype is intended to accommodate residential uses in a townhome building. Buildings are required to be attached, however, no fewer than two, and no more than four townhomes shall be attached without a sideyard or passageway. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and is set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a small-scale urban residential character, creating a gracious village ambience and a pedestrian-friendly environment. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley.
- ❑ **Use:** Residential, live/work townhomes and apartments, and community commercial.

- ❑ **Dwelling units per acre:** 12 units per acre maximum.
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 35 foot maximum height.
- ❑ **Lot size:** Varies. Minimum lot width for townhomes: 24 feet. Generally, blocks are 230 feet deep with a 20 foot wide alley right of way.
- ❑ **Lot Coverage.** No maximum.

❑ **Setbacks:**

Front setback. Minimum: 5 feet. Maximum: 20 feet. Preferred: 10 feet.

Side setback. 10 feet, except when abutting a residential zone, then the side setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.

Rear setback. Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1/2 foot for each foot by which the building exceeds 15 feet.

Rear garage setback if abutting an alley. 7.5 feet from rear lot line.

❑ **Parking and Garages:**

General. Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.

- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, stoops, bay windows, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.

- ❑ **Landscaping:** See Multiple-Family Rules & Design Guidelines.

- ❑ **Lighting:** See Multiple-Family Rules & Design Guidelines.

- ❑ **Signage:** See Multiple-Family Rules & Design Guidelines.

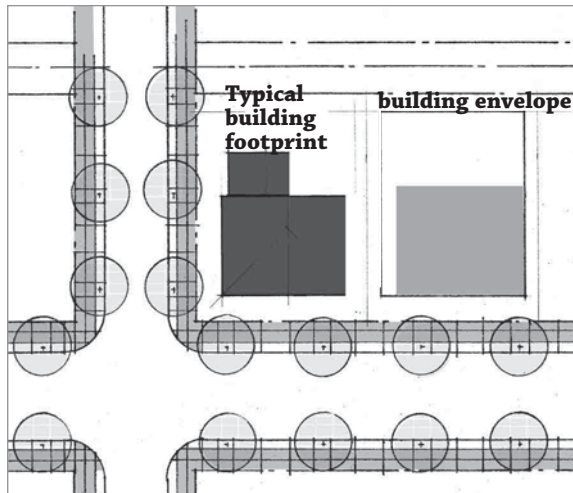
- ❑ **Base Zone:** Urban Standard Residential (RS).

- ❑ **Overlay District:** Residential Mixed-Use and Residential Multiple-Family.





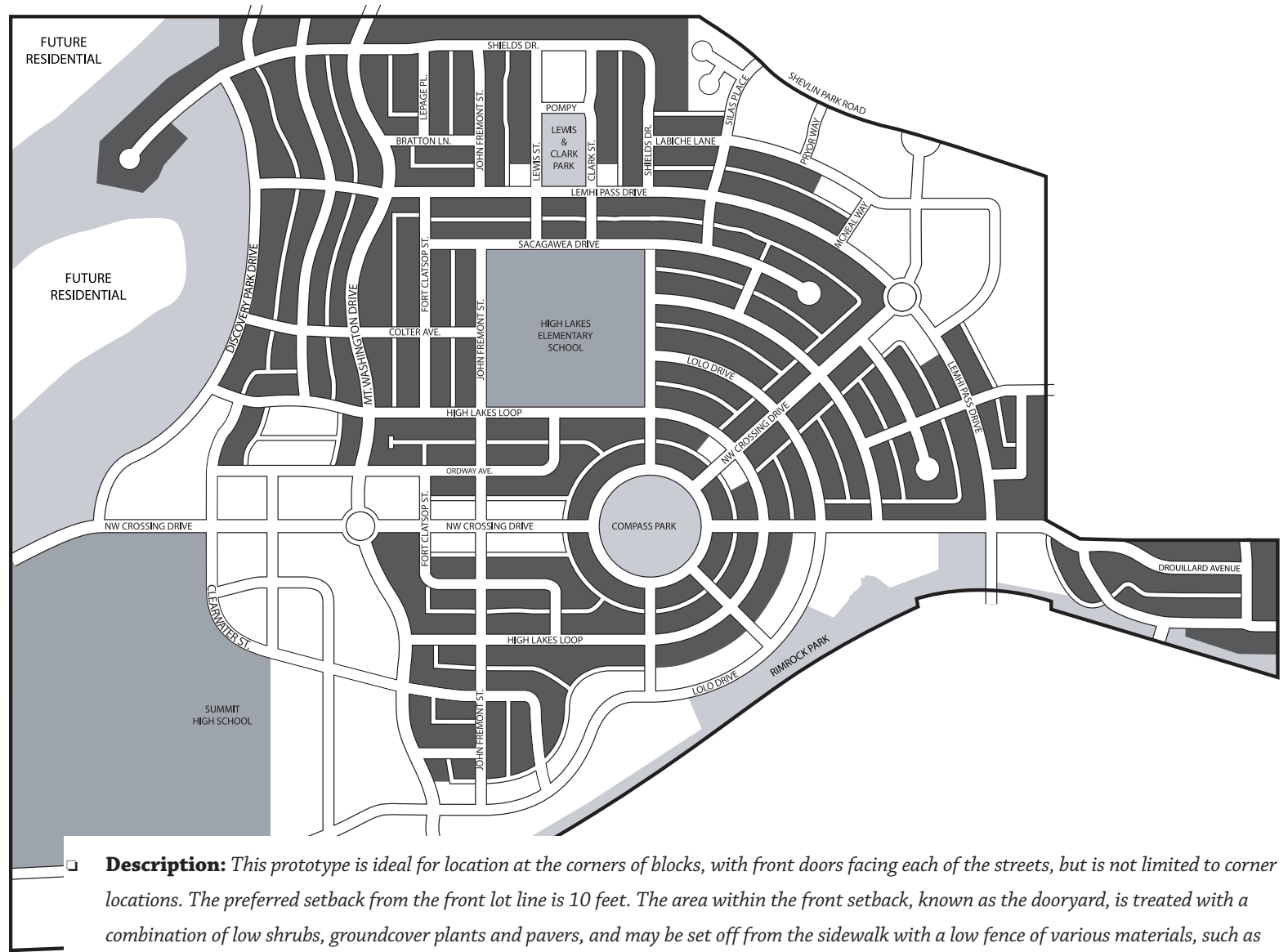
Axonometric view of typical building



Site Plan

VILLAGE DUPLEX. KEY MAP

Village Prototypes

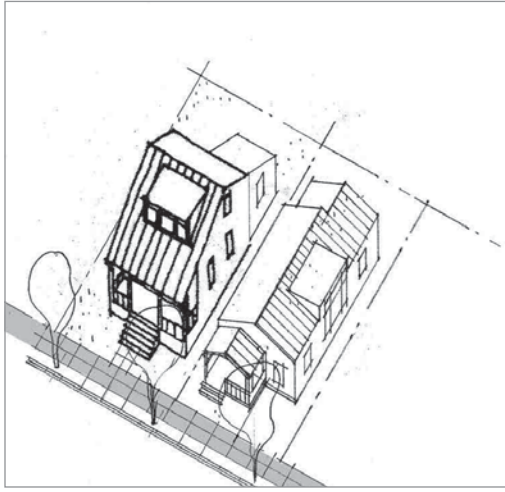


Description: This prototype is ideal for location at the corners of blocks, with front doors facing each of the streets, but is not limited to corner locations. The preferred setback from the front lot line is 10 feet. The area within the front setback, known as the dooryard, is treated with a combination of low shrubs, groundcover plants and pavers, and may be set off from the sidewalk with a low fence of various materials, such as wrought iron, stone, masonry or wood. Buildings will be mostly two stories fronting the street with a detached single-family dwelling character. The Village Duplex prototype is intended to look like a detached single dwelling, even while it provides two dwelling units, and is intended to provide a graceful transition between the more massive, attached two and four-townhomes and the Neighborhood prototypes. Street-facing facades will be a combination of tall gable ends, a flat roof behind a parapet, a “false front,” or a roof sloping to the street with prominent gable dormers. Parking is located at the rear of the lot, off of an alley. This prototype applies to only lots platted as duplex lots.

Use: Residential.

- ❑ **Dwelling units per acre:** 12 maximum
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 30 foot maximum height.
- ❑ **Lot size:** 8,000 square feet minimum.
- ❑ **Lot Coverage.** 35%.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet. Maximum: 20 feet. Preferred: 10 feet.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 10 feet.
 - Rear garage setback if abutting an alley.** 5 feet from rear lot line, plus 1 foot for each foot by which the building exceeds 15 feet.
- ❑ **Parking and Garages:**
 - General:** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street:** Parking spaces required: 1 per dwelling unit.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, stoops, bay windows, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** Not allowed.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** None.



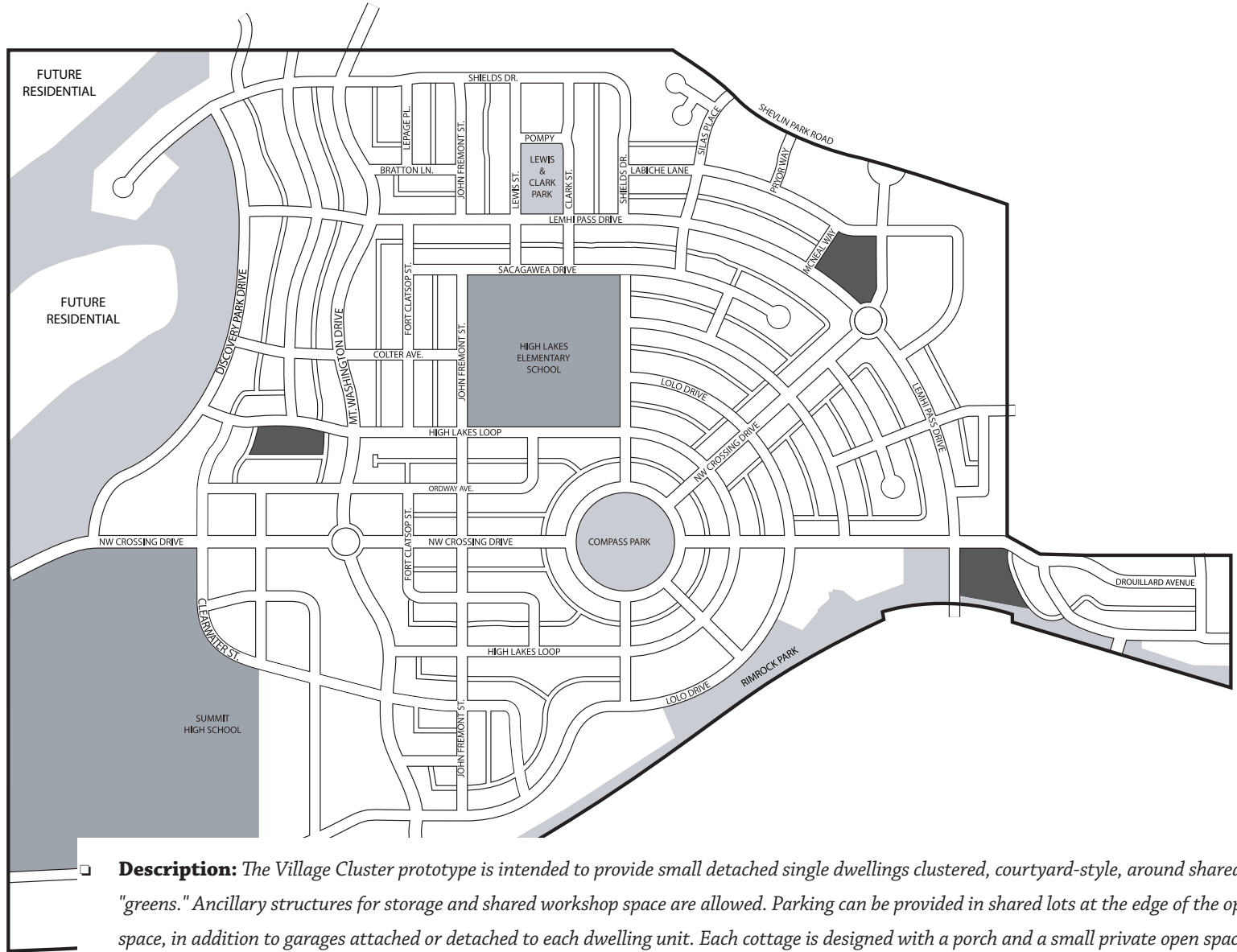


Axonometric view of typical building



Typical
building
footprint

Site Plan



□ **Description:** The Village Cluster prototype is intended to provide small detached single dwellings clustered, courtyard-style, around shared "greens." Ancillary structures for storage and shared workshop space are allowed. Parking can be provided in shared lots at the edge of the open space, in addition to garages attached or detached to each dwelling unit. Each cottage is designed with a porch and a small private open space, in addition to the shared open space. Overall, this prototype is intended to provide modest homes, but without sacrificing the outdoor area and storage/work area that homeowners expect, and that typically represent an advancement over apartment living. This prototype is designated for areas where block size and configuration is not typical, and more unusual building arrangements are necessary. Because the open space, driveways and parking areas are shared. Buildings may be developed as condominiums.

□ **Use:** Residential.

- ❑ **Dwelling units per acre:** 12 maximum
- ❑ **Accessory dwelling unit:** Not allowed.
- ❑ **Height:** 25 foot maximum height.
- ❑ **Lot size:** 2,000 square feet minimum.
- ❑ **Lot Coverage.** No maximum.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet if abuts a public right-of-way. Maximum: 20 feet.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
- ❑ **Parking and Garages:**
 - Off-street.** Parking spaces required: 1 minimum or 2 maximum per dwelling unit.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, stoops, bay windows, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** Not allowed.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** Residential Cluster.

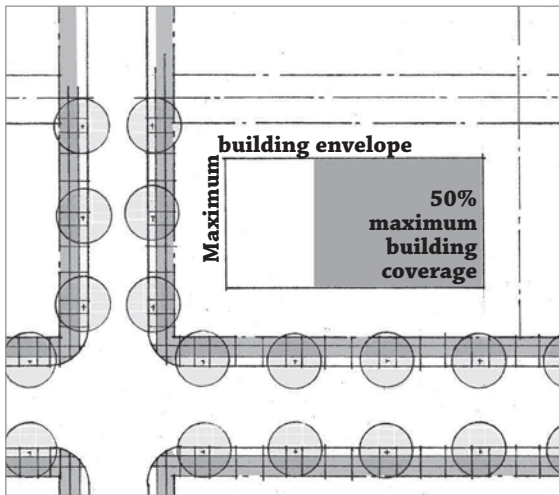


VILLAGE CLUSTER. SPECIFICATIONS

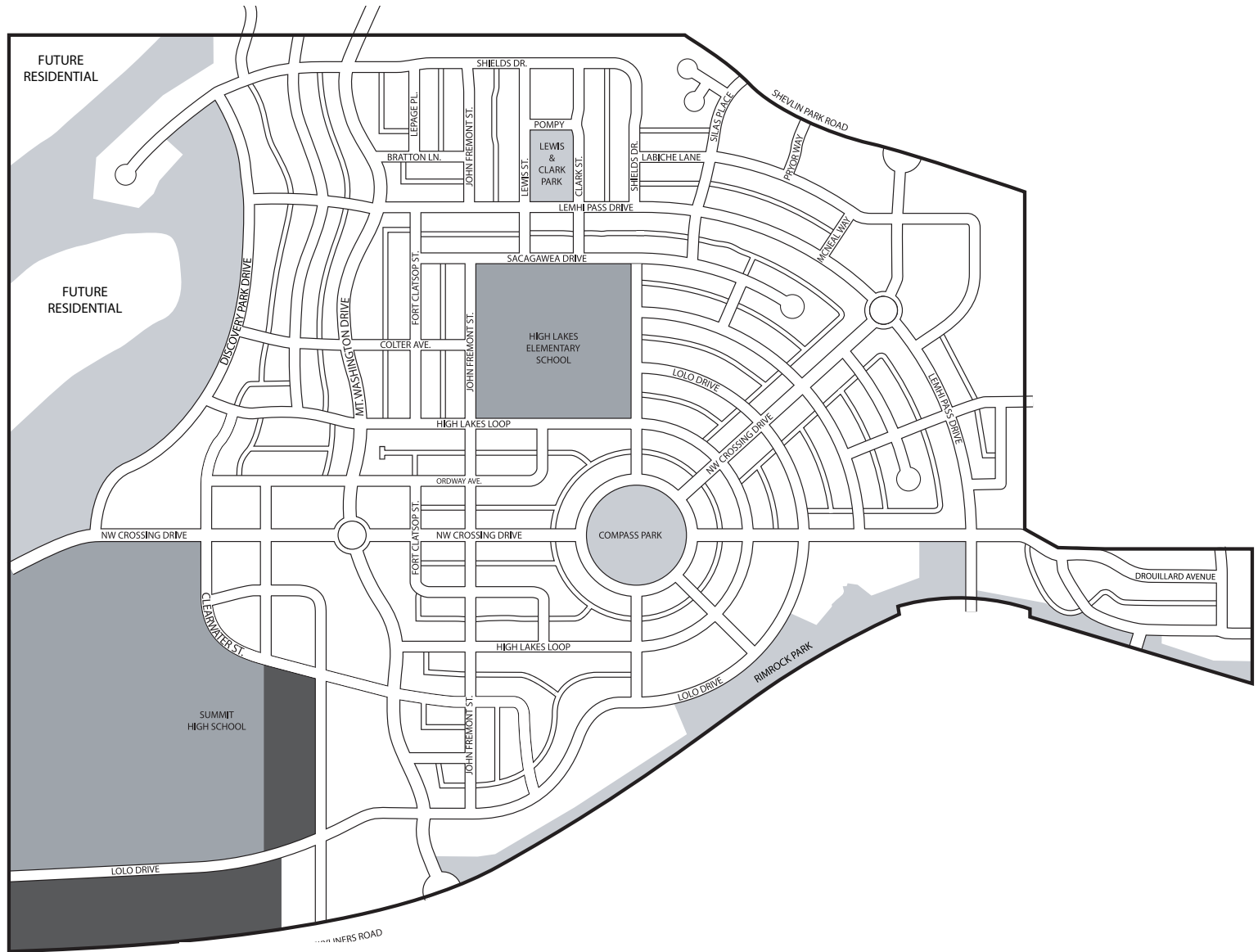
Village Prototypes

V.6

VILLAGE INDUSTRIAL. KEY MAP



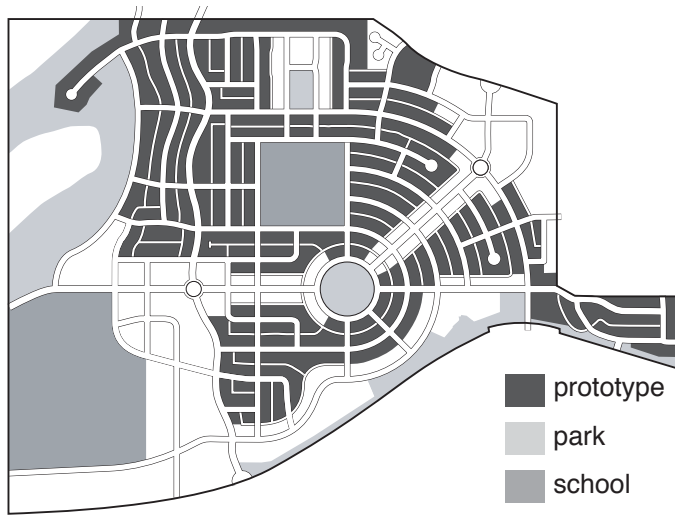
Site Plan



- ❑ **Description:** The Village Industrial Prototype is designed for buildings up to two stories in an office park environment. The preferred setback from the front lot line is 20 feet. Parking is allowed to the sides of buildings, although the preferred location for parking is at the rear of the lot.
- ❑ **Use:** All uses permitted in City of Bend Industrial Employment Overlay District, NorthWest Crossing Overlay Zone, Wholesale, R&D, light manufacturing and assembly, printing, professional office, lumber and building material yard, not including concrete mixing.

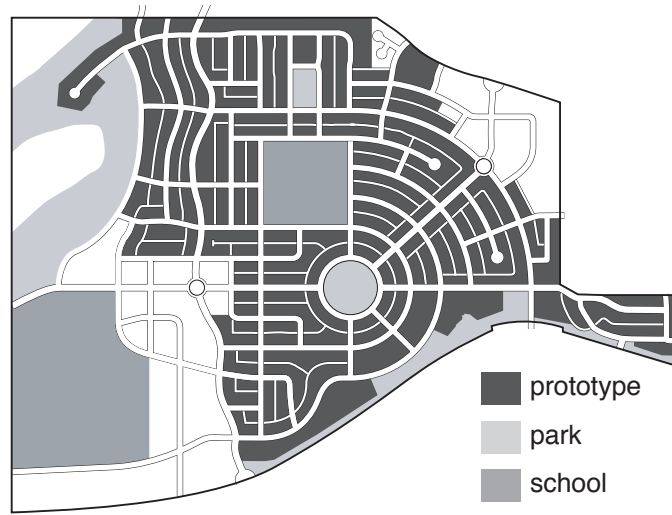
- ❑ **Dwelling units per acre:** *Not applicable.*
- ❑ **Accessory dwelling unit:** *Not applicable.*
- ❑ **Height:** *45 foot maximum height, higher with City of Bend conditional use permit.*
- ❑ **Lot size:** *Varies. Minimum lot area: 1/2 acre.*
- ❑ **Lot Coverage.** *50% maximum.*
- ❑ **Setbacks:**
 - Front setback.** *Minimum: 20 feet. Maximum: no maximum. Preferred: 20 feet.*
 - Side setback.** *15 feet.*
 - Rear setback.** *5 feet.*
- ❑ **Parking:**
 - Off-street:** *As per City of Bend code parking requirements for use. Parking located at side or rear of building.*
- ❑ **Encroachments:** *Within the front setback, covered entries, awnings and stairs, etc., are encouraged. At the second level of buildings, encroachments in the form of balconies and bay windows are encouraged, but shall not exceed 5 feet or the depth of the setback.*
- ❑ **Landscaping:** *See Commercial Rules & Design Guidelines.*
- ❑ **Lighting:** *See Commercial Rules & Design Guidelines.*
- ❑ **Signage:** *See Commercial Rules & Design Guidelines.*
- ❑ **Base Zone:** *Light Industrial.*
- ❑ **Overlay District:** *Industrial Employment.*





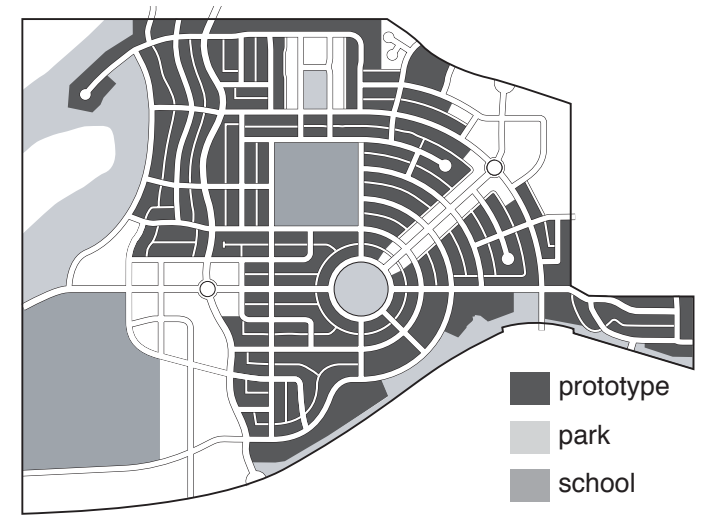
N.1 Neighborhood Small Lot

Description: The Neighborhood Small Lot prototype is the smallest of the Neighborhood prototypes. Residential buildings are detached, on lots ranging from 4,000 to 5,999 square feet. Ridges run perpendicular to the street, so that gable ends face the street or, if parallel, prominent gable dormers face the street. The preferred setback from the front lot line is 10 feet. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, in detached or attached garages.



N.2 Neighborhood Medium Lot

Description: The Neighborhood Medium Lot prototype is intended to provide a variety amongst the Neighborhood Small Lot and the Neighborhood Large Lot prototypes. Residential buildings are detached, on lots 6,000 to 7,999 square feet. The preferred setback from the front lot line is 10 feet. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, in detached or attached garages.

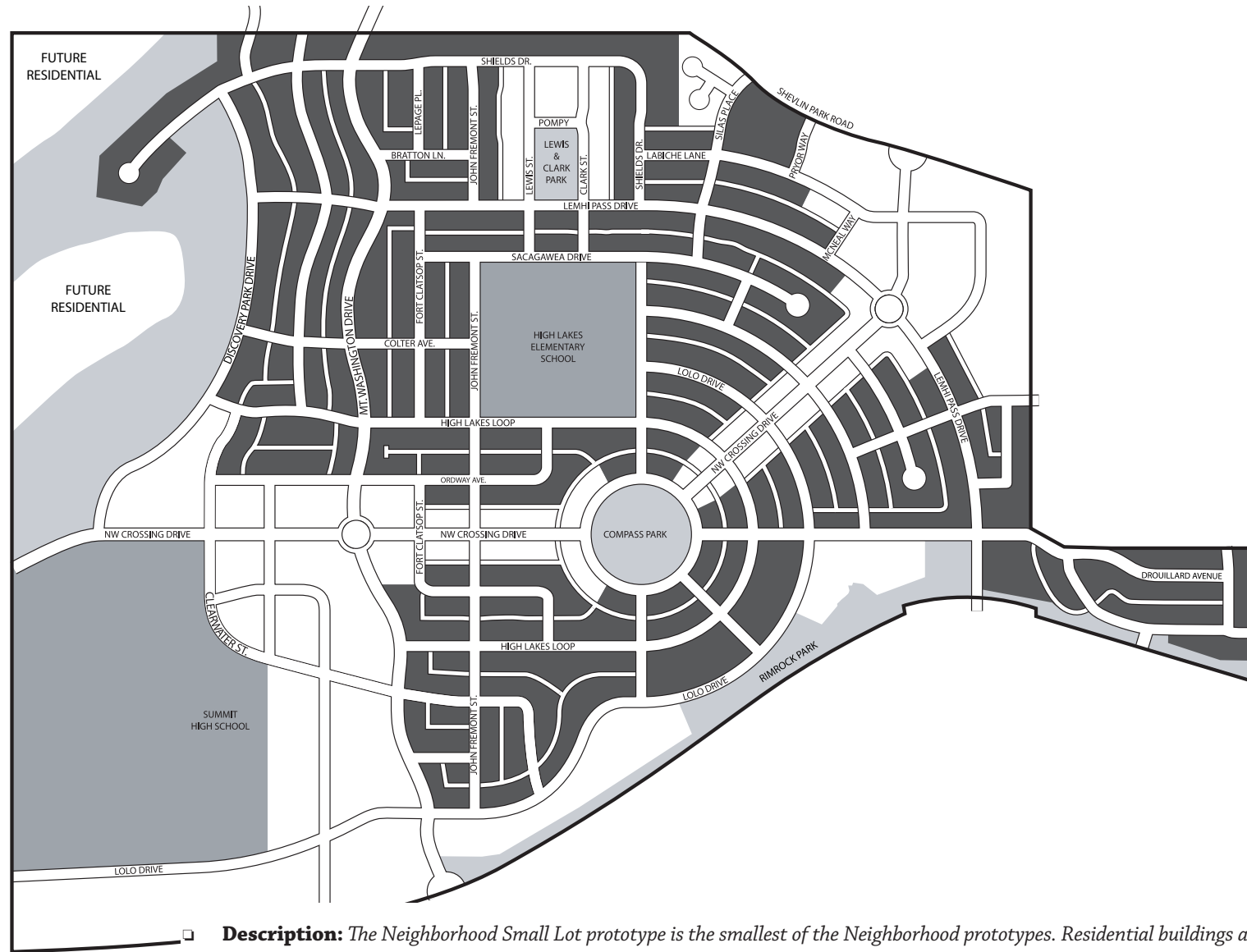


N.3 Neighborhood Large Lot

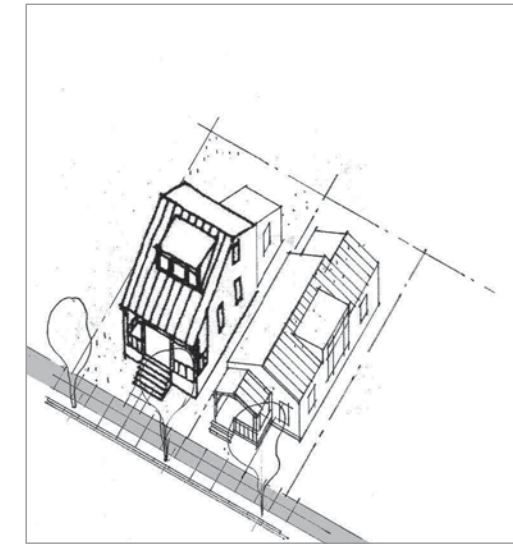
Description: The Neighborhood Large Lot prototype is the largest of the Neighborhood prototypes. Residential buildings are detached, on lots 8,000 square feet or greater. Buildings are more horizontal and less massive at the street facing facade. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, or accessed from a front or side driveway in detached or attached garages.

LOCATION SUMMARY. ALL NEIGHBORHOOD PROTOTYPES

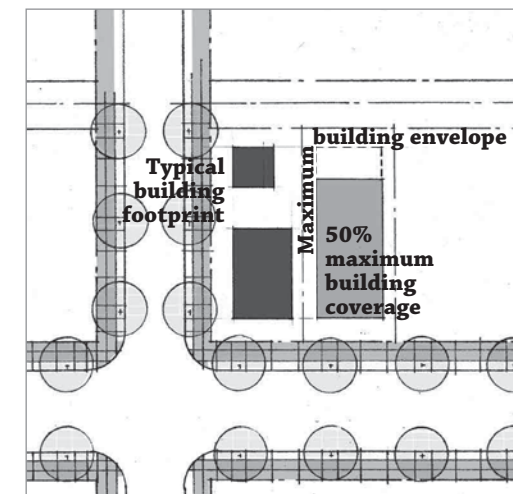
Neighborhood Prototypes



- ❑ **Description:** The Neighborhood Small Lot prototype is the smallest of the Neighborhood prototypes. Residential buildings are detached, on lots ranging from 4,000 to 5,999 square feet. Ridges run perpendicular to the street, so that gable ends face the street or, if parallel, prominent gable dormers face the street. The preferred setback from the front lot line is 10 feet. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, in detached or attached garages.
- ❑ **Use:** Residential.



Axonometric view of typical building



Site Plan

NEIGHBORHOOD SMALL LOT. KEY MAP

Neighborhood Prototypes

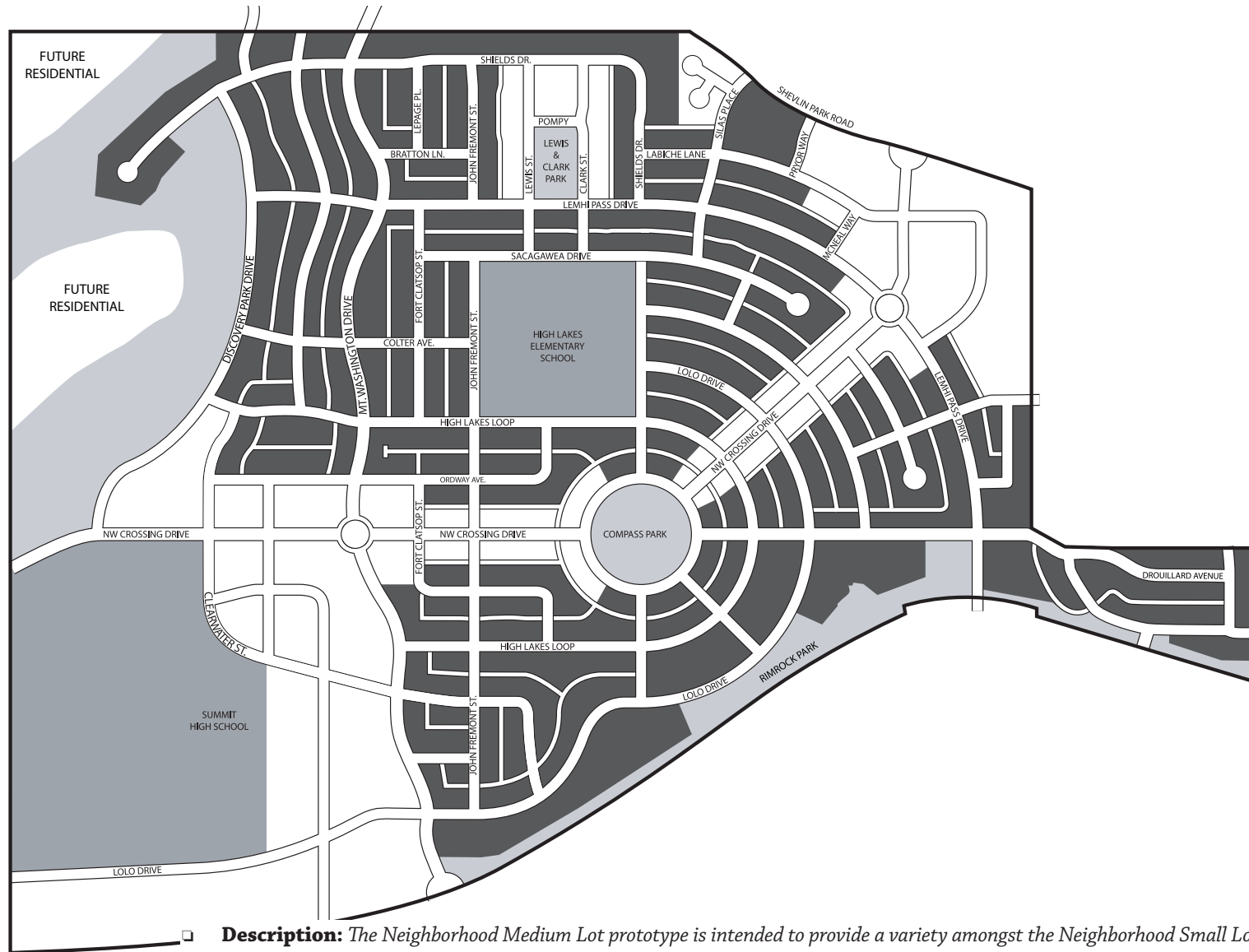


N.1

NEIGHBORHOOD SMALL LOT. SPECIFICATIONS

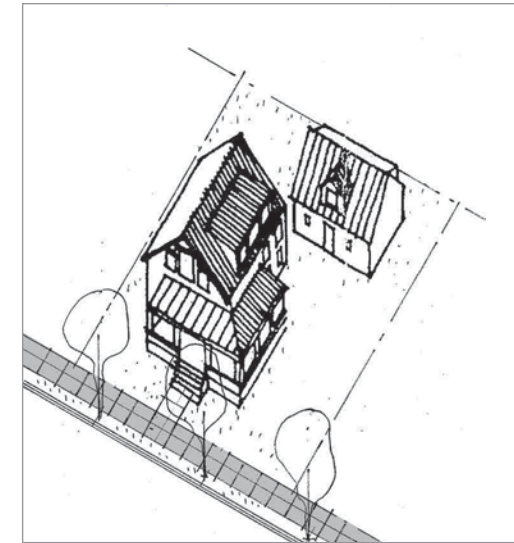
Neighborhood Prototypes

- ❑ **Dwelling units per acre:** 7.3 maximum
- ❑ **Accessory dwelling unit:** Allowed.
- ❑ **Height:** 30 foot maximum height.
- ❑ **Lot size:** Varies. 4,000 to 5,999 square feet.
- ❑ **Lot Coverage.** 50% maximum.
- ❑ **Building Size Limitations. (F.A.R.)** Maximum house square footage including garage shall not exceed 48% of lot area. This massing restriction is calculated based upon the total square feet of the building including areas with heights of 5 feet or higher. Garage space is limited to two cars. Porches, decks, and basements are excluded. Refer to the Residential Rules & Design Guidelines.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet. Maximum: 20 feet. Preferred: 10 feet.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 5 feet .
 - Rear garage setback if abutting an alley.** Setback shall be 5 feet plus 1 foot for each foot by which the building exceeds 15 feet.
 - Front garage setback.** The garage shall be a minimum of 32 feet back from front face of house.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. A maximum of two parking bays is allowed within a garage. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** Parking spaces required: 2 per dwelling unit.
 - On-street.** Permitted.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, and bay windows, etc., when allowed by the City of Bend, are encouraged.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** See Residential Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** None.

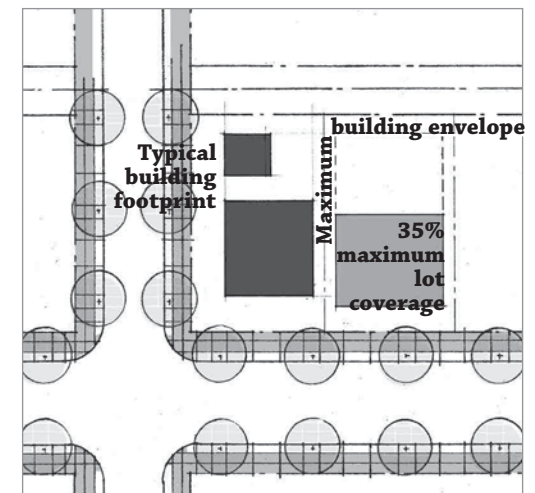


- ❑ **Description:** The Neighborhood Medium Lot prototype is intended to provide a variety amongst the Neighborhood Small Lot and the Neighborhood Large Lot prototypes. Residential buildings are detached, on lots 6,000 to 7,999 square feet. The preferred setback from the front lot line is 10 feet. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, in detached or attached garages.

- ❑ **Use:** Residential.



Axonometric view of typical building



Site Plan

NEIGHBORHOOD MEDIUM LOT. KEY MAP

Neighborhood Prototypes

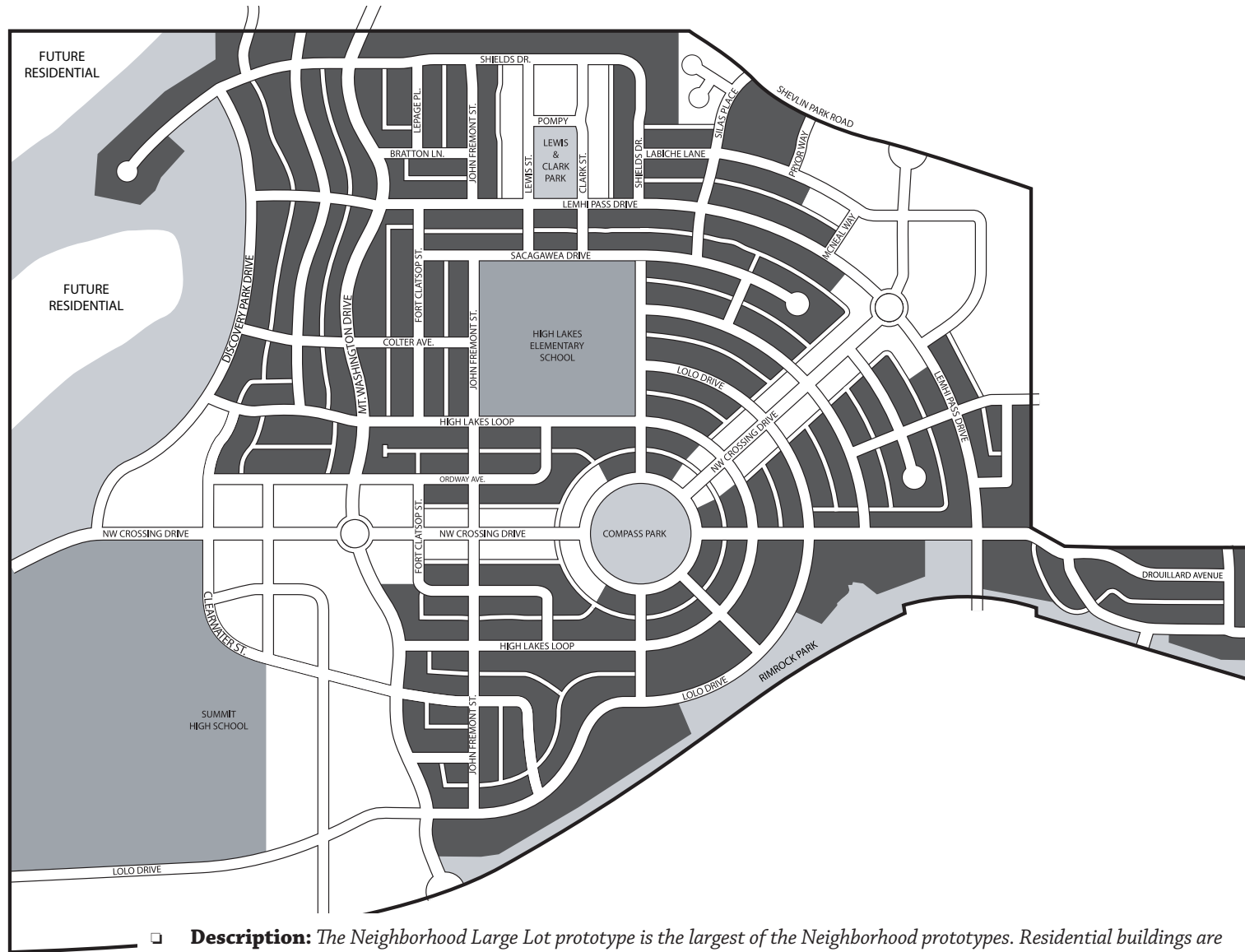
N.2

NEIGHBORHOOD MEDIUM LOT. SPECIFICATIONS

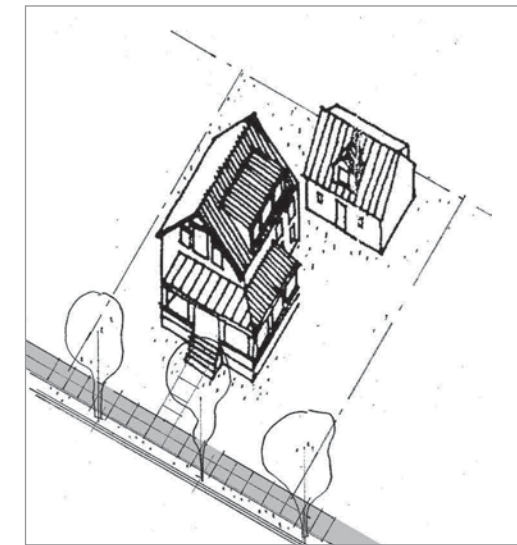
Neighborhood Prototypes



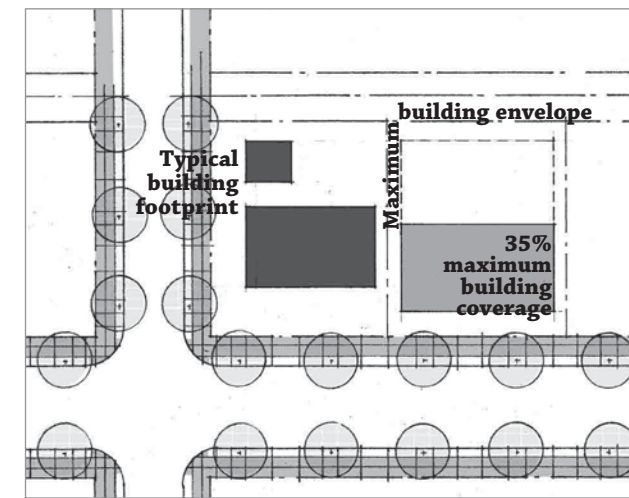
- ❑ **Dwelling units per acre:** 5 maximum
- ❑ **Accessory dwelling unit:** Allowed.
- ❑ **Height:** 30 foot maximum height.
- ❑ **Lot size:** Varies. 6,000 to 7,999 square feet.
- ❑ **Lot Coverage.** 35% maximum.
- ❑ **Building Size Limitations. (F.A.R.)** Maximum house square footage, including garage, shall not exceed 48% of lot area. This massing calculation is calculated based upon the total square feet of the building, including areas with heights of 5 feet or higher. Porches, decks, and basements are excluded. Refer to the Residential Rules & Design Guidelines.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet. Maximum: 20 feet. Preferred: 10 feet.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 5 feet.
 - Rear garage setback if abutting an alley.** Setback shall be 5 feet plus 1 foot for each foot by which the building exceeds 15 feet.
 - Front garage setback.** The preferred garage setback shall be a minimum of 32 feet back from front face of house. A third bay on the garage shall be set back a minimum of 4 feet back from front face of the garage. If existing topography, trees, or rock outcroppings impact the 32 foot garage setback, a 10 foot garage setback from the front face of house with architectural enhancements to de-emphasize the garage. Or, the garage can be perpendicular to the street with at least a 10 foot front setback with architectural enhancements.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** Parking spaces required: 2 per dwelling unit.
 - On-street.** Permitted
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, and bay windows, etc., when allowed by the City of Bend, are encouraged.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** See Residential Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** None.



- ❑ **Description:** The Neighborhood Large Lot prototype is the largest of the Neighborhood prototypes. Residential buildings are detached, on lots 8,000 square feet or greater. Buildings are more horizontal and less massive at the street facing facade. Porches and other encroachments into the setback, allowed by the City of Bend, are encouraged. Accessory dwelling units are allowed. Off-street parking is typically located at the rear of the lot, off of an alley, or accessed from a front or side driveway in detached or attached garages.
- ❑ **Use:** Residential single-family detached.



Axonometric view of typical building



Site Plan

NEIGHBORHOOD LARGE LOT. KEY MAP

Neighborhood Prototypes

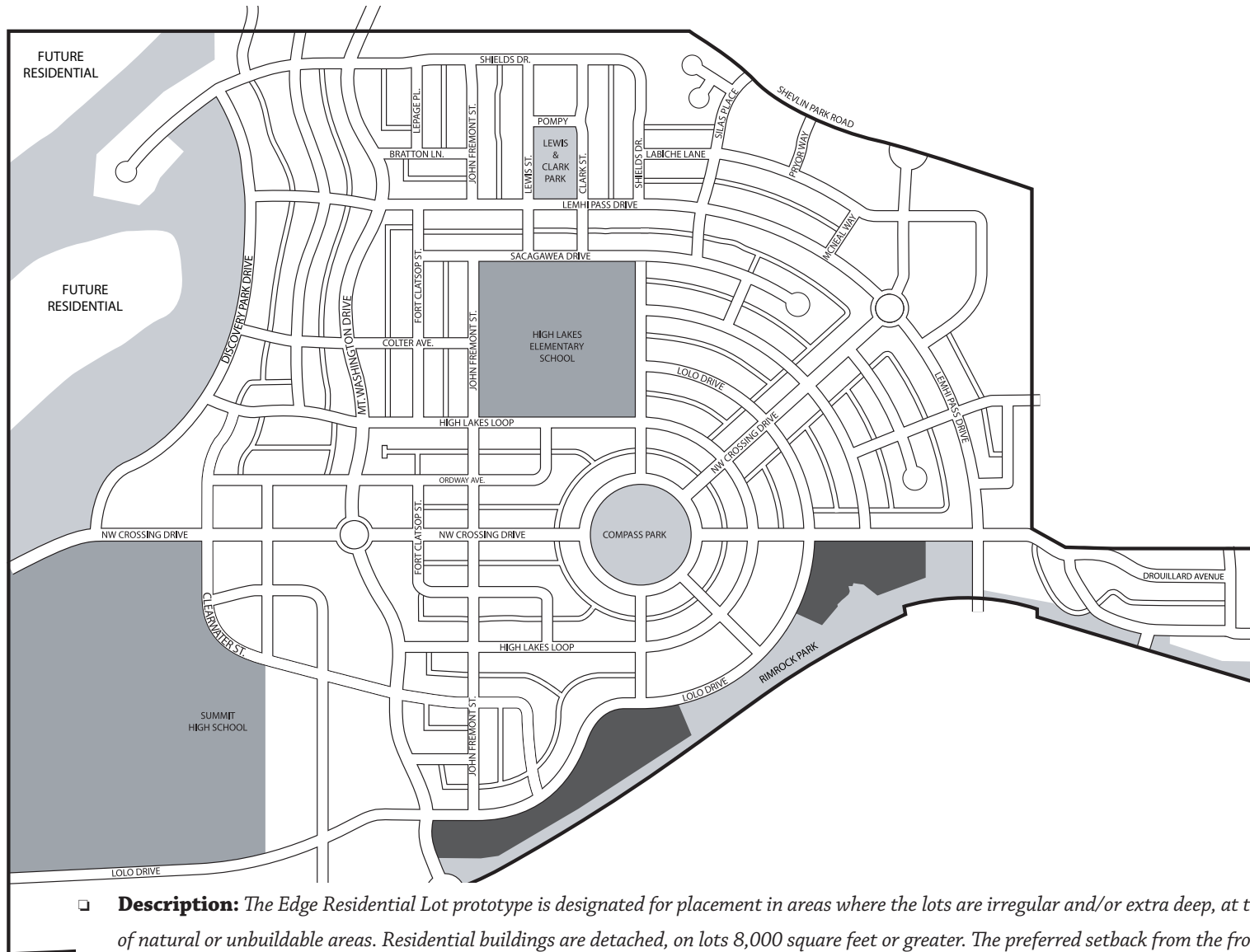


N.3

NEIGHBORHOOD LARGE LOT. SPECIFICATIONS

Neighborhood Prototypes

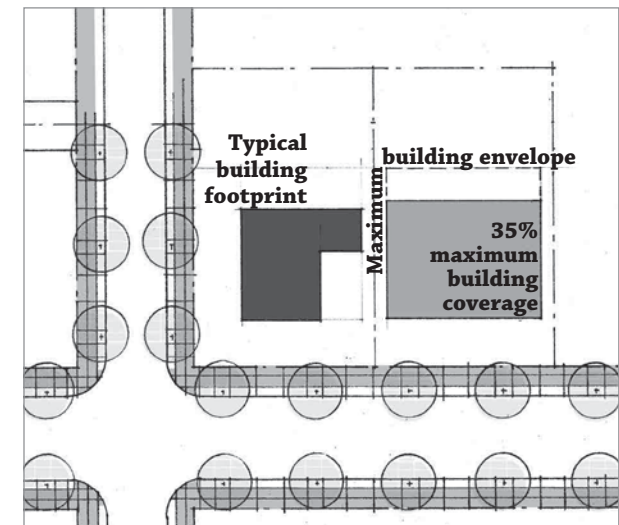
- ❑ **Dwelling units per acre:** 4 maximum.
- ❑ **Accessory dwelling unit:** Allowed.
- ❑ **Height:** 30 foot maximum height.
- ❑ **Lot size:** Varies. 8,000 square feet or greater.
- ❑ **Lot Coverage.** 35% maximum.
- ❑ **Building Size Limitations. (F.A.R.)** Maximum house square footage, including garage, shall not exceed 48% of lot area. This massing calculation is calculated based upon the total square feet of the building, including areas with heights of 5 feet or higher. Porches, decks, and basements are excluded. Refer to the Residential Rules & Design Guidelines.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet. Maximum: no maximum. Preferred: no preferred.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 5 feet.
 - Rear garage setback if abutting an alley.** Setback shall be 5 feet plus 1 foot for each foot by which the building exceeds 15 feet.
 - Front garage setback.** The preferred garage setback shall be a minimum of 32 feet back from front face of house. A third bay on the garage shall be set back a minimum of 4 feet back from front face of the garage. If existing topography, trees, or rock outcroppings impact the 32 foot garage setback, a 10 foot garage setback from the front face of house with architectural enhancements to de-emphasize the garage. Or, the garage can be perpendicular to the street with at least a 10 foot front setback with architectural enhancements.
- ❑ **Parking and Garages:**
 - General.** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** Parking spaces required: 2 per dwelling unit.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, and bay windows, etc., when allowed by the City of Bend, are encouraged.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** See Residential Rules & Design Guidelines.
- ❑ **Base Zone:** Urban Standard Residential (RS).
- ❑ **Overlay District:** None.



- ❑ **Description:** The Edge Residential Lot prototype is designated for placement in areas where the lots are irregular and/or extra deep, at the edge of natural or unbuildable areas. Residential buildings are detached, on lots 8,000 square feet or greater. The preferred setback from the front lot line is 10 feet. Porches and other encroachments into the setback are encouraged. Accessory dwelling units are allowed. Buildings are more horizontal and less massive at the street facing facade. Ridges run parallel to the street, and shed dormers or small gable dormers face the street. Accessory dwelling units are allowed. Off-street parking is typically accessed from a front or side driveway, in detached or attached garage structures.
- ❑ **Use:** Residential single-family detached.



Axonometric view of typical building



Site Plan

EDGE RESIDENTIAL. KEY MAP

Edge Prototypes

E.1

EDGE RESIDENTIAL. SPECIFICATIONS

Edge Prototypes



- ❑ **Dwelling units per acre:** 4 maximum.
- ❑ **Accessory dwelling unit:** Allowed.
- ❑ **Height:** 30 foot maximum height.
- ❑ **Lot size:** Varies. 8,000 square feet or greater.
- ❑ **Lot Coverage.** 35% maximum.
- ❑ **Building Size Limitations. (F.A.R.)** Maximum house square footage, including garage, shall not exceed 48% of lot area. This massing calculation is calculated based upon the total square feet of the building, including areas with heights of 5 feet or higher. Porches, decks, and basements are excluded. Refer to the Residential Rules & Design Guidelines.
- ❑ **Setbacks:**
 - Front setback.** Minimum: 10 feet. Maximum: no maximum.
 - Side setback.** 5 feet.
 - Rear setback.** Minimum: 5 feet. When abutting a residential zone, setback shall be increased 1 foot for each foot by which the building exceeds 15 feet.
 - Rear garage setback if abutting an alley.** 7.5 feet from rear lot line.
 - Front garage setback.** The preferred garage setback shall be a minimum of 32 feet back from front face of house. A third bay on the garage shall be set back a minimum of 4 feet back from front face of the garage. If existing topography, trees, or rock outcroppings impact the 32 foot garage setback, a 10 foot garage setback from the front face of house with architectural enhancements to de-emphasize the garage. Or, the garage can be perpendicular to the street with at least a 10 foot front setback with architectural enhancements.
- ❑ **Parking and Garages:**
 - General:** Parking and garages shall be located off of the alleys or in the interior of the site. Exceptions to allow parking at the side shall be conditional based on review and approval of the NorthWest Crossing Architectural Review Committee.
 - Off-street.** Parking spaces required: 2 per dwelling unit.
- ❑ **Encroachments:** Within the front setback, porches, covered entries, stairs, and bay windows, etc., are encouraged.
- ❑ **Landscaping:** See Residential Rules & Design Guidelines.
- ❑ **Lighting:** See Residential Rules & Design Guidelines.
- ❑ **Signage:** Not allowed.